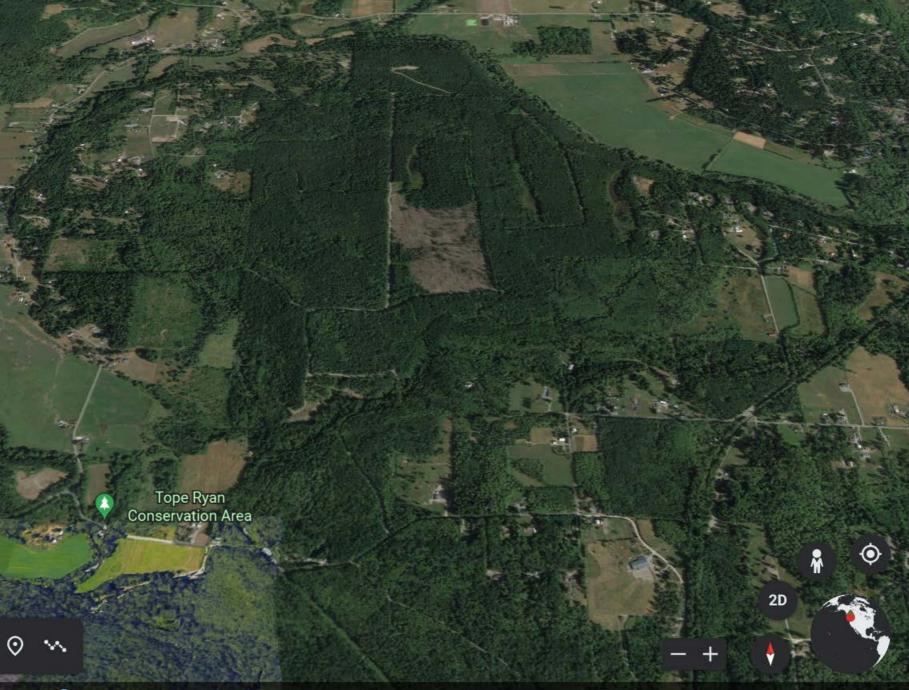


Hearing Examiner

August 26, 2022; August 29, 2022; September 2nd, 8th, 9th, & 13th, 2022

Presentation By: Kevin Cricchio, AICP, ISA, Senior Planner

Planning & Development Services Department













SUBJECT PROPOSAL:

PL16-0097, Special Use Permit Application PL16-0098, Forest Practice Conversion Application

On March 7, 2016, Skagit County Planning & Development Services received an application for a Special Use Permit (**File: PL16-0097**) to permit a proposed gravel mine/quarry on the subject property. The proposed gravel mine would remove approximately 4,280,000 cubic yards of gravel from three parcels (P125644, P125645, & P50155) over a period of approximately 25 years. The three parcels total approximately 77 acres, of which 68 acres will be cleared, however the mining will occur on approximately 51 acres of the 68-acre cleared area. Gravel will be removed from the site by truck and trailer, generating an average of 46 trips per day, and the material will be transported to market or to one of Concrete Nor'West's nearby facilities for processing.

SUBJECT PROPOSAL (Continued):

The site is accessed from Grip Road on an existing private gravel haul road located approximately 0.70 miles east of the Prairie Road/Grip Road intersection. Operations onsite will be limited to excavation. No processing is proposed onsite. The applicant is proposing to haul material from 7:00 AM to 5:00 PM Monday through Friday.

In addition to the Special Use Permit application, the applicant also applied for a Forest Practice Conversion application (**File: PL16-0098**). To facilitate the proposed mining/quarry operation onsite, the applicant proposes to harvest approximately 50,000 board feet of timber on 68 acres, remove the stumps, and convert the parcels to a gravel mining operation.

SUBJECT PROPOSAL (Continued):

Both applications were determined to be complete on March 22, 2016 and a letter of completeness was issued for the applications.

PROJECT LOCATION:

The proposed properties subject to the mining operation are located approximately 1.5 miles north of Grip Road and south/southwest of the Samish River, within a portion of the Southeast Quarter of Section 27; Township 36 North; Range 04 East; Willamette Meridian within unincorporated Skagit County, Washington.

SUBJECT PARCELS:

Proposed Mine/Quarry: P125644, P125645, & P50155

<u>Haul Road (Under Same Ownership Providing Access to Mine)</u>: P125646, P125647, P125626, P125627, P125628, P125629, P125630, P125631, P125623, P125624, P125632, P125633, & P35704

<u>Contiguous Subject Parcels (Under Same Ownership for Noticing Purposes)</u>: P125648, P125649, P50087, P125618, P125634, P125640, P125619, P125635, P125641, P125620, P125636, P125642, P125621, P125637, P125643, P125622, P125638, P125639, & P125625

APPLICANT:

Concrete Nor'West / Miles Sand & Gravel C/O: Dan Cox, General Manager P.O. Box 280 Mount Vernon, Washington 98273

LANDOWNER:

Lisa Inc. Attention: Concrete Nor'West 400 Valley Avenue Northeast Puyallup, Washington 98372

AGENT /LAND SURVEYOR:

John Semrau, PLS, PE Semrau Engineering & Surveying 2118 Riverside Drive, Suite 208 Mount Vernon, Washington 98273

APPLICANT'S ATTORNEY:

William T. Lynn and Reuben Schutz Gordon Thomas Honeywell 600 University, Suite 2100 Seattle, Washington 98101

SITE DESCRIPTION:

The project area is approximately 68 acres in size and consists of a sand & gravel terrace above the Samish River Valley. The terrace consists of glacial outwash deposit composed of gravelly sand with some silt. This formation represents the southern extent of a glacial recessional outwash complex deposited during the Sumas Stade of the Fraser glaciation period. The glacial formation at this location is unconformably underlain by older Vashon Stade glacial advance outwash.

Topographically, the northeast corner of the property is at an elevation of approximately 200 feet above mean sea level. The elevation rises to an elevation of approximately 280 feet along the southwest boundary of the proposed mining area for a site relief of 80 feet.

SITE DESCRIPTION (Continued):

The hydrogeology report indicates the groundwater table elevation is approximately 145 near the northern portion of the site to 155 feet near the southern boundary of the parcel. The groundwater down gradient direction is to the north to northeast. The finished elevation of the base of the mine, as indicated on Figure 3 of the Hydrogeology report, is 165 feet near the Southwest corner of the site to 155 feet near the northeast corner. No structures exist on the site presently and the site is currently used for timber production.

ZONING DISTRICT & COMPREHENSIVE PLAN LAND USE DESIGNATION:

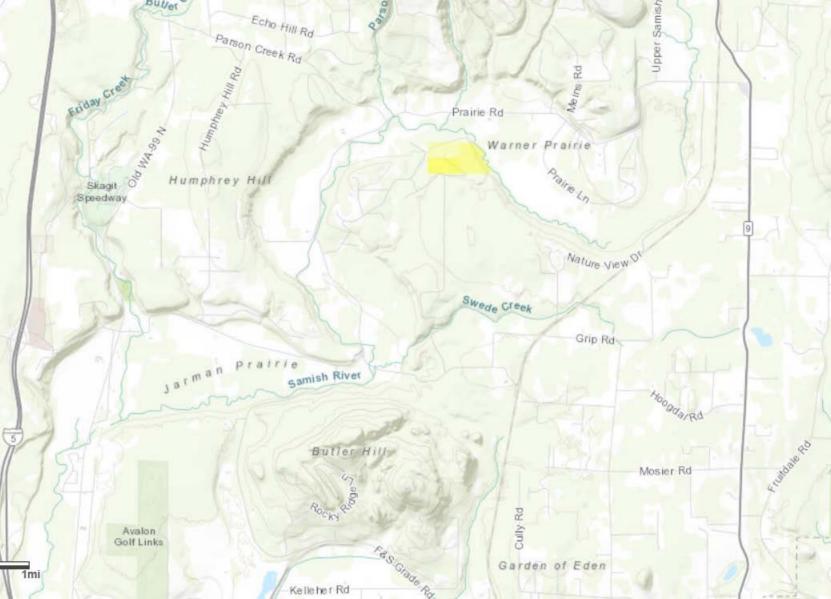
The subject properties lie in the Rural Resource-Natural Resource Lands (RRc-NRL) Zoning District & Comprehensive Plan Land Use Designation of Skagit County.

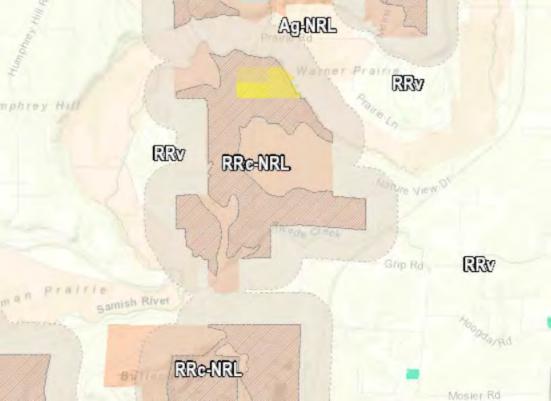
MINERAL RESOURCE OVERLAY (MRO):

The subject parcels (P125644, P125645, & P50155) where the gravel mine/ quarry is proposed, is located in a Mineral Resource Overlay.

SURROUNDING ZONING & LAND USES:

Direction:	Zoning:	Present Land-Use:
North:	Rural Resource Natural Resource Lands & Agricultural Natural Resource Lands	Forestry, Sporadic Single-Family Residences, & Farms/ Agriculture
South:	Rural Resource Natural Resource Lands	Forestry & Sporadic Single-Family Residences
East:	Rural Resource Natural Resource Lands, Agricultural Natural Resource Lands, & Rural Reserve	Forestry, Sporadic Single-Family Residences, & Farms/ Agriculture
West:	Rural Resource Natural Resource Lands, Rural Reserve, Agricultural Natural Resource Lands	Forestry, Sporadic Single-Family Residences, & Farms/ Agriculture







ACREAGE:

According to Skagit County Assessors records, the 3 subject parcels where mining is proposed (P50155 = 37-acres; P125644 = 20 acres; P125645 = 20-acres) cumulatively total 77 acres.

According to the applicant's submitted narrative and SEPA environmental checklist, a 68-acre sand and gravel mine is proposed within the 3 subject parcels identified above.

The SEPA environmental checklist (page 4) further states that mining will be within a 51 acre portion of the site.

ACREAGE (Continued):

Project Description:

Consistent with both <u>Assessors records and the applicant's application</u> <u>material submitted</u>, the issued SEPA MDNS (2022) stated that "The three parcels total approximately 77 acres, of which 68 acres will be cleared, however the mining will occur on approximately 51 acres of the 68-acre cleared area

ACREAGE (Continued):

Applicant's Corrected Acreage /Land Survey:

On March 11, 2022, Skagit County's Planning & Development Services Department received a letter (See Exhibit 3) from the applicant stating that the acreage that was listed in the issued (2022) SEPA MDNS consisting of 77 acres for the subject mine is incorrect. Instead of 37 acres which the Assessor's records indicates Parcel P50155 being, it more accurately is 29.6 acres in size. Additionally, parcels P125644 and P125645 are said to be more accurately 19.60 acres in size (for a total of 39.20 acres) instead of 20 acres per parcel per Assessor's records. These corrected acreages are based on a land survey of the subject property the applicant had performed.

Applicant's Corrected Acreage /Land Survey (Continued):

The applicant further states, that the project description more accurately should state that the acreage of the mine is approximately 68 acres with an area to be cleared, mined, and reclaimed at 51 acres.

Staff Analysis:

The applicant's corrected acreages for the 3 subject parcels involved in the proposed sand and gravel mine is consistent with what was noticed and listed in the issued (2022) SEPA MDNS.

SHORELINE JURISDICTION:

The Samish River flows along the eastern border of the project site. There are shoreline associated riparian wetlands that have been delineated. The <u>shoreline environment</u> designation for this portion of the Samish River is <u>**Rural**</u>.

All proposed mining activities will be located outside of shoreline jurisdiction. No development is being proposed within shoreline jurisdiction and thus no shoreline permit is required. The subject Special Use Permit has been conditioned below accordingly

CRITICAL AREAS REVIEW:

As part of the submittal, the applicant included a "**Hydrogeologic Site Assessment & Maps**" (See Exhibit 9) that was prepared by: Associated Earth Sciences, dated August 21, 2015. According to this report, the following conclusions were set as follows:

- "Ground water is present beneath the site at a depth of approximately 70 feet, at an elevation of approximately 145 to 155 feet (msl).
- Ground water beneath the site generally flows to the north and/or northeast and presumably discharges to Samish River, located a minimum of 200 feet from the proposed mine boundary.

CRITICAL AREAS REVIEW:

• There are no water supply wells located down gradient of the proposed mine, between the mine boundary and the Samish River.

• The proposal is for a dry, surface mining operation, with limited on-site processing. The potential for a negative impact to ground water quantity and/or quality from the proposed mining activities is low."

CRITICAL AREAS REVIEW:

<u>Geologically Hazardous Area / Steep Slopes:</u>

The applicant submitted a "**Response to Skagit County Geologic Hazard Requirement**" (See Exhibit 10) regarding geologic hazards of the haul road that was prepared by: Associated Earth Sciences, dated: December 16, 2021. According to this geologic report, the following conclusion and recommended mitigation is as follows:

"The proposed gravel mine will utilize the existing haul road to transport aggregate from the mine to Grip Road and will propose asphalt surfacing for a portion of the haul road north of Swede Creek. This section of haul road is located near a geologic hazard area which includes landslide and erosion hazards. No alteration through grading, stripping, or mining to the geologic hazard areas is proposed.

CRITICAL AREAS REVIEW:

Geologically Hazardous Area /Steep Slopes (Continued):

The change in use of the existing gravel-surfaced road from a logging haul road to an aggregate mine haul road is considered by the County a form of site disturbance, and therefore is subject to the County's critical areas review. From a geotechnical standpoint, the haul road will have a similar function and will be subject to similar truck loads compared to its past use. Review of the geologic mapping indicates the steep slope areas have a core of dense glacially consolidated sediments, which is consistent with our on-site observations. No evidence of significant slope instability, road instability, past deep-seated land sliding, or groundwater seepage, was observed on the steep slopes near the haul road or on the haul road itself. This is also consistent with review of LIDAR imagery of the site. Given these factors, it is our opinion that the impact to the identified geologic hazards near the haul road caused by the change in haul road usage based on truck type can be avoided. This conclusion is predicated on the project following the mitigation recommendations..."

CRITICAL AREAS REVIEW:

Geologically Hazardous Area /Steep Slopes (Continued):

"Mitigation for using the existing haul road near the geologic hazard areas for mining

operation shall include:

- 1. No clearing of vegetation within the geologic hazard areas.
- 2. Maintain roadside swales and check dams. Clean out material that has

sloughed into the swale that could potentially block surface water. Avoid concentrated surface water discharge onto the steep slopes.

3.Do not place uncontrolled fill, strippings, or other debris over the top of steep slopes."

CRITICAL AREAS REVIEW:

Wetlands / Fish & Wildlife Habitat Assessment:

As part of the submittal, the applicant included a "Samish River Ordinary High Water Mark (OHWM) /Wetland Edge" determination (See Exhibit 4) that was prepared by: Graham-Bunting Associates, dated: May 18, 2015. The report concluded that "It is our opinion that the area of shoreline management jurisdiction extends 200-feet landward of the OHWM as identified in the field and depicted on the site plan prepared by Semrau Engineering and Surveying."

The applicant further submitted a "Fish and Wildlife Site Assessment" (See Exhibit 5) prepared by Graham-Bunting Associates, dated: August 20, 2015.

CRITICAL AREAS REVIEW:

Wetlands / Fish & Wildlife Habitat Assessment (Continued):

On page 7, this report provided reasoning for allowing use of the moderate land-use intensity buffer rather than the high land-use intensity buffer pursuant to SCC 14.24.240(3)(a). The measures proposed to mitigate the impacts from the proposed mine include:

- The mine site is located greater than 200 horizontal feet landward of the OHWM
- The mine site is also separated vertically approximately 90-feet above the OHWM
- Mining activities will be separated from the OHWM by a protective berm
- The dry mine floor will maintain a maximum depth of 10-feet above the underlying water table

CRITICAL AREAS REVIEW:

Wetlands / Fish & Wildlife Habitat Assessment (Continued):

- All surface water will drain through the gravel floor of the mine site no surface water will drain directly to the Samish River.
- The mine site is located in an area logged during the 1990s by a previous landowner.
- No processing or industrial activity is proposed in conjunction with the project.
- Aggregate extraction will be maintained at a relatively low volume level.
- The project will utilize an interior road system
- The area contiguous to the berm will be mined first and reclaimed pursuant to a reclamation plan to be approved by the Washington State Department of Natural Resources. The goal of the reclamation plan will be to return the site to forest management or low density residential which are considered low and moderate land use intensities respectively.

CRITICAL AREAS REVIEW:

Wetlands / Fish & Wildlife Habitat Assessment (Continued):

The Assessment recommended the following mitigation measures:

1. The Samish River and associated wetland should be provided with the optional 200 foot buffer required for Type S Waters of the state and Category II wetlands with moderate intensity land uses and a habitat score of 30.

2. The buffer should be measured on a horizontal plane landward of the OHWM/associated wetland edge located at the toe of slope east of the proposed mine site.

3. The riparian buffer and associated wetland and 200-foot buffer should be designated as a Protected Critical Area to assure identification and long term protection. The site plan included as Attachment C is prepared in a format suitable for recording with the Skagit County Auditor.

4. Because the riparian and associated wetland buffers are also coexistent with the jurisdictional area regulated under the Shoreline Management Act (90.58 RCW) and Shoreline Master Program (14.26 SCC), consultation with Skagit County Planning and Development services should occur prior to initiating the application process for a Special use Permit.

CRITICAL AREAS REVIEW:

Addendum to Fish & Wildlife Site Assessment:

On April 18, 2017, the applicant submitted an "<u>Addendum to Fish & Wildlife Site</u> <u>Assessment</u>," (Exhibit 6) prepared by Graham-Bunting Associates. According to this Addendum:

"With the exception of identification of critical habitat associated with the [Oregon Spotted Frog] OSF, no additional endangered, threatened or sensitive species or habitats were identified. Application of the standard riparian buffer requirement established under SCC 14.24.530 is generally recognized as the preferred method of avoiding project impacts consistent with the mitigation sequence listed under SCC 14.24.080-(5)-(b). In our opinion establishment of a no disturb buffer of 200 feet is sufficient to protect aquatic life in the river and associated wetland.

CRITICAL AREAS REVIEW:

Addendum to Fish & Wildlife Site Assessment (Continued):

Lacking a request for additional biological information from a federal agency of jurisdiction, it is our opinion that the analysis prepared by GBA and submitted by the applicant provides an appropriate level of detail to address Skagit County Code requirements."

CRITICAL AREAS REVIEW:

Haul Road Critical Area Assessment (Wetland Delineation & Fish & Wildlife Habitat Conservation Area [HCA]) and Impact Assessment & Mitigation Plan:

In December 2021, the applicant submitted <u>both</u> a "<u>Critical Area Assessment (Wetland</u> <u>Delineation & Fish & Wildlife Habitat Conservation Area)</u>" and "Impact Assessment & <u>Mitigation Plan</u>" (Exhibit 8) prepared by Northwest Ecological Services" (NES) with regard to the haul road. The Assessment states as follows:

"The project does not include any direct wetland, stream, or buffer impact. Therefore, traditional mitigation measures such as wetland or buffer enhancement have not been presented. The use of the haul road to transport material from the proposed gravel mine will result in a greater number of trips per day compared to the existing forestry practice. This may potentially result in indirect impacts to water quality and potentially wildlife functions associated with site critical areas or their buffers.

CRITICAL AREAS REVIEW:

Continued - Haul Road Critical Area Assessment (Wetland Delineation & Fish & Wildlife Habitat Conservation Area [HCA]) and Impact Assessment & Mitigation Plan:

For the reasons previously mentioned these indirect impacts are anticipated to be minor. However, the <u>following mitigation measures have been recommended</u>:

• During resurfacing- establish erosion control and BMP's to ensure protection of downstream waters.

• Haul trucks shall be maintained in good working condition such that petroleum products or other harmful chemicals are not leaked into adjacent critical areas.

• During operation of the mine, maintain existing forested vegetation adjacent to the roadway, particularly in wetlands and buffers. This forested buffer along the road provides water quality filtration of surface waters prior to entering adjacent wetlands and streams and provides a buffer and screening for wildlife using the interior of the site.

• Review the proposal for compliance with applicable County/ State stormwater management requirements."

CRITICAL AREAS REVIEW:

Floodplain:

The proposed mining area is not located within a flood hazard zone.

EMPLOYEES:

The applicant has indicated that up to 1 to 2 employees would be working onsite during maximum operation. No offices or buildings for the employees are proposed to be constructed onsite. Potable water for employees will be brought onsite from offsite sources.

RESTROOMS:

Restrooms for employees that work onsite will be provided by portable sanitation facilities.

PROPOSED HOURS & DAYS OF OPERATION:

The applicant proposes that the days and hours of operation will generally be limited to Monday through Saturday, from dawn till dusk. The applicant proposes that the hours of operation maybe expanded based on market conditions and seasonal demands. Skagit County Code 14.16.440 (10)(i) requires that hours of operation vary according to the zoning designation of the site but may be shortened by the Hearing Examiner based on site-specific circumstances.

PROPOSED HOURS & DAYS OF OPERATION (Continued):

The proposed mine is located within the Rural Resource-Natural Resource Land (RRc-NRL) designated area. Mining operations on RRc-NRL designated land may be granted unlimited hours of operation. However, the Hearing Examiner may limit hours of operation to daylight hours or to such other reasonable limitation deemed necessary to address potential significant adverse impacts to existing adjacent land uses, on any portion of the mining site where mining activity is proposed to occur less than one quarter mile from existing Rural Intermediate, Rural Village, or Urban Growth Area designated lands. The proposed mining site is located greater than one quarter mile from any Rural Intermediate, Rural Village and Urban Growth Area designated lands.

PDS LIMITATION ON HOURS & DAYS OF OPERATION:

In order to mitigate potential impacts of the quarry on the neighborhood, the hours of operation were limited by Planning & Development Services Department in the issued SEPA MDNS. The hours were limited to Monday through Friday from 7:00 AM to 5:00 PM. No mining operations are permitted outside of these times including holidays. If seasonal (temporary) demand indicates a need for extended hours, or Saturday or Sunday operations, the applicant shall submit a request for a temporary deviation to these permitted hours to Planning & Development Services (PDS). If permitted by PDS, such operations may be subject to additional conditions by PDS.

PROJECT ACCESS:

The proposed mining site is located approximately 1.5 miles north of Grip Road. As illustrated by plan Sheet C1 (See Exhibits 19 & 40), access to the proposed mining area from Grip Road is on an existing private gravel forestry road extending north from Grip Road. The forest practice road will be upgraded as necessary to meet Skagit County's private road standards.

INTERNAL BRIDGE:

The haul road used for ingress and egress to the proposed mine site crosses over an approximate 14 foot by 40 foot privately owned bridge that spans Swede Creek (See Exhibits 19 & 40). The bridge has been evaluated for loads associated with the proposed sand and gravel mine and deemed sufficient to support this operation (See Exhibits 20 & 21).

TRAFFIC IMPACT ANALYSIS:

Based on the traffic impact analysis (TIA) that was submitted by the applicant, Skagit County-road standard level of service (LOS) requirements are met for each intersection impacted by the traffic generated by the Grip Road mine. No mitigation Level of Service measures were required based on the Traffic Impact Analysis report. However, to mitigate traffic related sight distance issues, a flashing beacons and signing system were proposed and agreed upon for traffic generated by the Grip Road mine. The recommendations listed in the TIA were incorporated as mitigation measures in the issued SEPA MDNS (See Exhibit 27) and conditions of approval for the subject permit conditions of approval /mitigation measures below

TRAFFIC:

The traffic impact analysis prepared for the gravel pit operation assumed 150,000 yards of gravel would be processed from the pit on an annual basis which would require an estimated 5,883 full trucks from the mine per year. Truck trips generated by the proposal is anticipated to average 23 full trucks (46 trips) per day or approximately 8 truck trips per hour, assuming hauling times would be limited to off peak hours, between 9:00 am and 3:00 pm (6 hours/day) for 260 workdays per year.

The applicant has indicated that there may be times in which market demands exceed average production. Under high market demand conditions, the gravel mining operation is proposed to be increased, but not to exceed the county road standard level of service (LOS) C.

TRAFFIC (Continued):

In order to maintain the LOS C, the maximum operation limit may not exceed 30 full trucks (60 trips) per hour with a maximum operation limit of 720 full truck trips per day (24 hour workday). During such conditions, we recommend that Concrete Nor'West notify Skagit County Public Works at least two weeks before pit operations are forecast to exceed the 46 full trucks per day. This will allow Public Works to issue an informational public notice of the increased operation to warn the public of the growth in truck traffic and determine if any additional roadway traffic measures are necessary.

THIRD PARTY REVIEW OF TRAFFIC IMPACT ANALYSIS:

On behalf of Skagit County, <u>consultants GTC and HDR performed third party review</u> (See Exhibits 15 & 16) of the applicant's traffic information, memorandums, and analysis. Their recommendations resulted in revisions to the applicant's traffic analysis /memos and ultimately in the submittal of a Traffic Impact Analysis (See Exhibit 18). The recommendations listed in the TIA were incorporated as mitigation measures in the issued SEPA MDNS (See Exhibit 27) and conditions of approval listed for the subject permit as listed below.

During the SEPA MDNS comment period, one comment was received from Kyle Loring, Loring Advising PLLC representing Central Samish Valley Neighbors. Included with this comment was who is a Traffic Impact Analysis Peer Review dated April 30, 2021 that was prepared by Transportation Solutions (See Exhibit 34).

PARKING:

Parking for employees, trucks, and operations vehicles will be available on-site. Traffic haul routes from the mine on public roads will include traveling west on Grip Road to Prairie Road, and then west on Prairie Road to Old Highway 99. Sand and gravel obtained from the Grip Road Mining site will be sold directly to market or processed at the existing Concrete Nor'West gravel mine processing operation located adjacent to Highway 99, south of the Samish River.

NOISE, EMISSIONS, & ODORS:

The applicant has indicated that the mining operation is anticipated to be in compliance with both day and night Skagit County noise regulations. The proposed use would not be expected to generate excessive emissions or odors with the exception of dust generation. The applicant has provided a Fugitive Dust Control Plan (Exhibit 22) which includes spraying water on roads and equipment to control fugitive dust.

The Department recommends that the applicant implement the Fugitive Dust Control Plan and during operations, if necessary, modify the plan as necessary to control dust emissions. At no time shall dust emissions exceed standards established by the Northwest Clean Air Agency (NWCAA).

SITE BUFFERS & SETBACKS:

Skagit County Code 14.16.440(10)(b)(iii) requires a 100-foot buffer be maintained around the perimeter of the mining site during mining operations. Once the extraction and transportation operations have been completed, the 100-foot buffer may be utilized for reclamation of the parcel. The applicant has volunteered a 50-foot setback from the mining area around the north, south, and west perimeter of the mining area. A 200-foot critical area buffer is required on the east perimeter of the subject site to protect wetlands and fish & wildlife habitat conservation areas. The 200-foot buffer will be designated as a protected critical area (PCA), through a PCA agreement with the applicant.

FINDINGS OF FACT:

- 1. The property proposed for development is zoned Rural Resource Natural Resource Lands (RR-NRL) as indicated in the Skagit County Comprehensive Plan and associated maps.
- 2. The subject properties are also a designated Mineral Resource Overlay (MRO).
- 3. <u>Gravel mines are allowed in RR-NRL/MRO with a hearing examiner</u> <u>special use permit</u> under SCC 14.16.430(4)(g).

PROJECT CHRONOLOGY:

- A. On March 7, 2016, PDS received applications for both a Special Use Permit and Forest Practice Conversion Permit.
- B. The subject applications were deemed technically complete on March 22, 2016, under SCC 14.06.100(2).
- C. A "Notice of Development Application" was published in the Skagit Valley Herald on March 31, 2016. This notice was posted onsite and mailed to neighboring landowners located within 300 feet of the subject properties. The public comment period ended on April 15, 2016.

PROJECT CHRONOLOGY:

- D. A SEPA Mitigated Determination of Non-Significance (MDNS) was issued on May 26, 2016.
- E. A "Notice of Public Hearing" was issued, and the matter went before the Hearing Examiner on November 16, 2016. PDS however determined that proper notice of the subject applications was not given. Accordingly, the Hearing Examiner opened the hearing but decided to continue it for a future date due to the need to perform proper noticing.
- F. PDS issued a second "Notice of Development Application" which was published on the Skagit Valley Herald on December 15, 2016. This notice was also posted onsite and mailed to neighboring landowners located within 300 feet of the subject property. The public comment period ended on December 30, 2016.

PROJECT CHRONOLGY:

G. The County received numerous comment from the public. Consequently, PDS requested additional information on March 14, 2017. The applicant provided some additional information, which PDS did not believe was sufficient or complete.

PDS made an Administrative Decision on April 5, 2018, to deny the subject applications for failure to timely submit requested information pursuant to SCC 14.06.105.

The applicant appealed this administrative decision (PL18-0200) on April 16, 2018.

On October 17. 2019, the Hearing Examiner denied the County's Motion for Summary Judgment, reversing PDS's denial, and ordering the application to go before the hearing examiner for decision on the merits.

PROJECT CHRONOLGY:

H. Following the appeal, the Applicant continued to provide additional information.

On April 15, 2021, PDS withdrew the MDNS and issued a new, more substantial, MDNS addressing the concerns raised about this particular project.

On May 11, 2021, the County withdrew the Second MDNS. That County action was not appealed.

PROJECT CHRONOLGY:

I. On June 17, 2021, Skagit County's Planning & Development Services Department informed the applicant that they were required to obtain Critical Areas review for the entire private haul road that would be used to access the proposed mine/quarry.

On June 24, 2021, an appeal (PL21-0348) from the applicant was received requesting the Hearing Examiner reverse this decision/ requirement by the county requiring critical area review of the haul road.

PROJECT CHRONOLGY:

J. <u>2022 SEPA MDNS</u>: A SEPA Mitigated Determination of Non-Significance (MDNS) was issued on February 22, 2022. This SEPA MDNS was published in the Skagit Valley Herald newspaper on February 24, 2022. Additionally, this MDNS was mailed and emailed to all parties of record and the applicant.

During the appeal period, one appeal (PL22-0142) of the issued SEPA MDNS was received on March 25, 2022, by Skagit County's Planning & Development Services Department.

PROJECT CHRONOLGY:

K. NOTICE OF PUBLIC HEARING: A Notice of Public Hearing was published in the Skagit Valley Herald newspaper on June 23, 2022, posted on the subject property, and mailed /emailed to <u>both landowners</u> located within 300 feet of the subject property as is required per Skagit County Code 14.06.150(3) <u>along</u> with parties of record that have commented on the project.

PROJECT CHRONOLGY:

L. <u>PUBLIC COMMENTS</u>: During the public comment periods associated with SEPA environmental review and the project as a whole, numerous comments were received. Most of the comments received however oppose the proposed development. These comments opposing the project can largely be categorized due to perceived <u>impacts to county roadways</u> from truck traffic from the gravel mine and <u>potential environmental degradation</u> associated with the proposed gravel mine/quarry.

Planning Department Staff believes that the issued SEPA MDNS and Special Use Permit and Forest Practice Conversion permits has been <u>conditioned</u> <u>accordingly to mitigate potential impacts to the environment, roads, and</u> <u>neighborhood that this project could pose</u>.

ROUTING:

- CRITICAL AREA & SHORELINE REVIEW: -Their comments/ requirements have become conditions of approval below and/or incorporated as mitigation measures in the issued SEPA MDNS.
- 2. PUBLIC HEALTH DEPARTMENT: Their comments/ requirements have become conditions of approval below and/or incorporated as mitigation measures in the issued SEPA MDNS.
- **3. PUBLIC WORKS DEPARTMENT REVIEW**: Their comments/ requirements have become conditions of approval below and/or incorporated as mitigation measures in the issued SEPA MDNS.
- 4. FIRE MARSHAL: No comment received

ROUTING:

5. **CURRENT PLANNING REVIEW**: The applicant is requesting a Special Use permit pursuant to Skagit County Code 14.16.430 Rural Resource-Natural Resource Lands (4)(g) and SCC 14.16.440 Mineral Resource Overlay (7). Hearing Examiner Special Use permits are processed as a Level II permit as outlined in SCC 14.06.120 Level II Review Procedures

Hearing Examiner special uses shall be reviewed as a Level II permit, pursuant to Chapter 14.06 SCC. The approving authority's decision may be to grant or to deny an application.

SPECIAL USE PERMIT CRITERIA OF APPROVAL:

Pages 19 - 27 of Staff Report

MINING OPERATIONS CRITERIA OF APPROVAL:

Pages 27-28 of Staff Report

HEARING EXAMINER REVIEW CRITERIA:

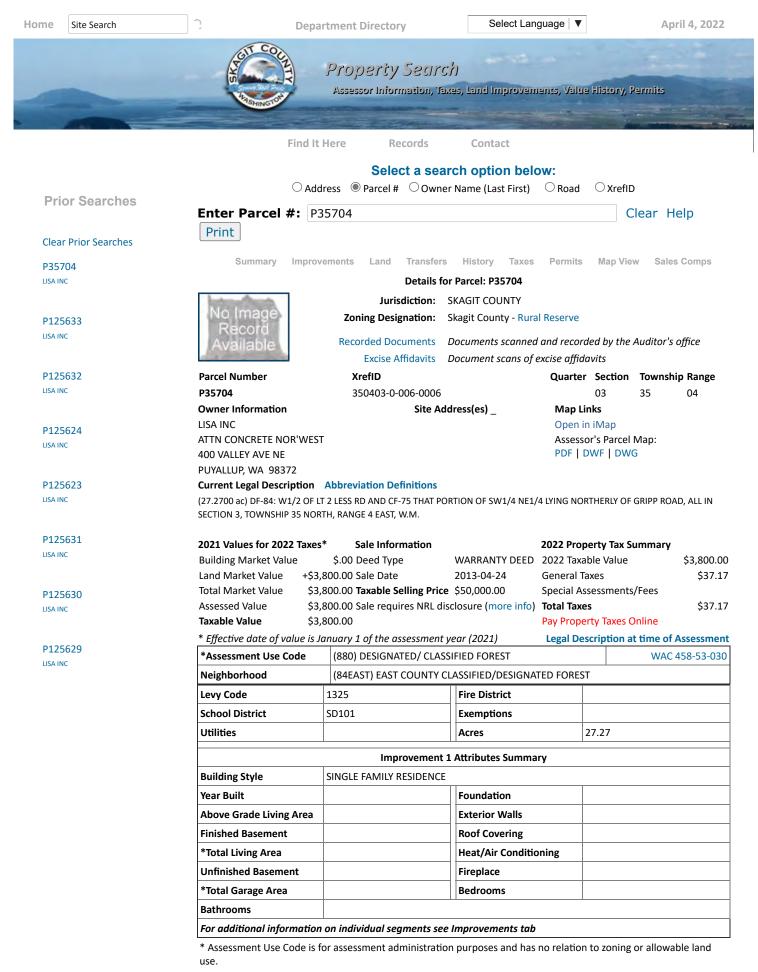
Pages 28-30 of Staff Report

RECOMMENDATION:

Based on a review of the application material submitted, special use criteria, environmental checklist, environmental studies, traffic impact analysis, issued SEPA Mitigated Determination of Non-Significance, and the above findings, staff recommends to the Hearing Examiner that the subject Special Use Permit & Forest Practice Conversion Permit applications be approved subject to conformance with the conditions of approval as listed below.

CONDITIONS OF APPROVAL:

Staff's suggested conditions of approval, pages 30-31 of staff report.



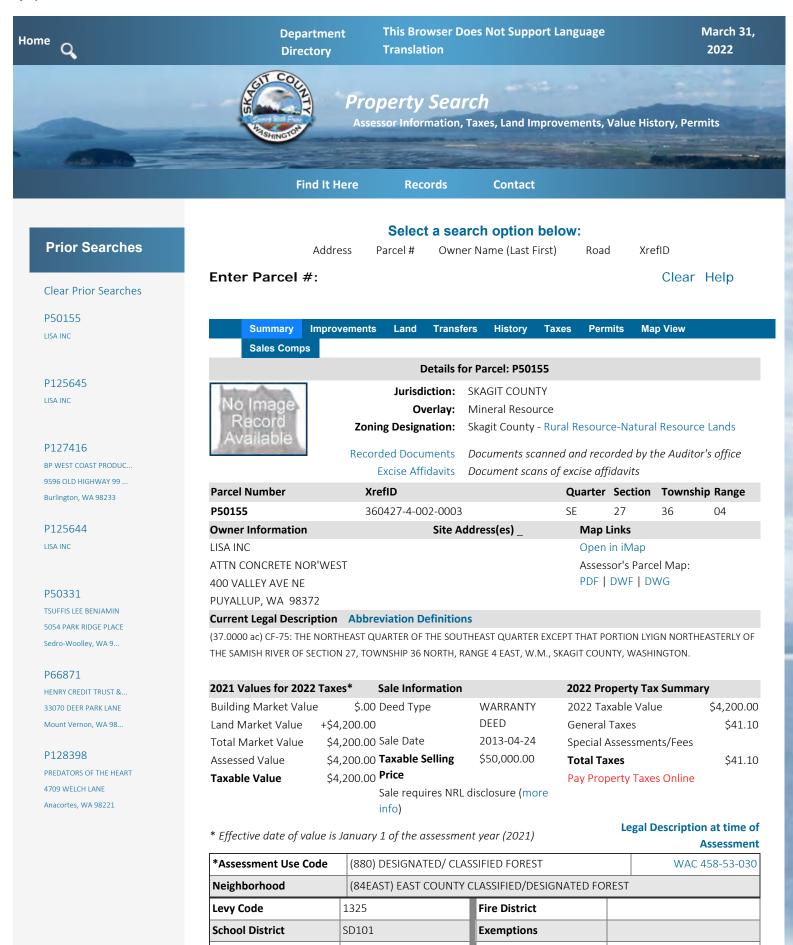
* Total living area includes above grade living area and finished basement area.

* Garage square footage includes all garage areas; basement garages, attached garages, detached garages, etc.

Assessment data for improvements is based on exterior inspections. Please contact the Assessor's office if the $_{Bac}$ information does not accurately reflect the interior characteristics.

Тор

Property Search



Improvement 1 Attributes Summary

Acres

37.00

Utilities

Building Style	SINGLE FAMILY RESIDENCE	
Year Built		Foundation
Above Grade Living Area		Exterior Walls
Finished Basement		Roof Covering
*Total Living Area		Heat/Air Conditioning
Unfinished Basement		Fireplace
*Total Garage Area		Bedrooms
Bathrooms		
For additional information on individual segments see Improvements tab		

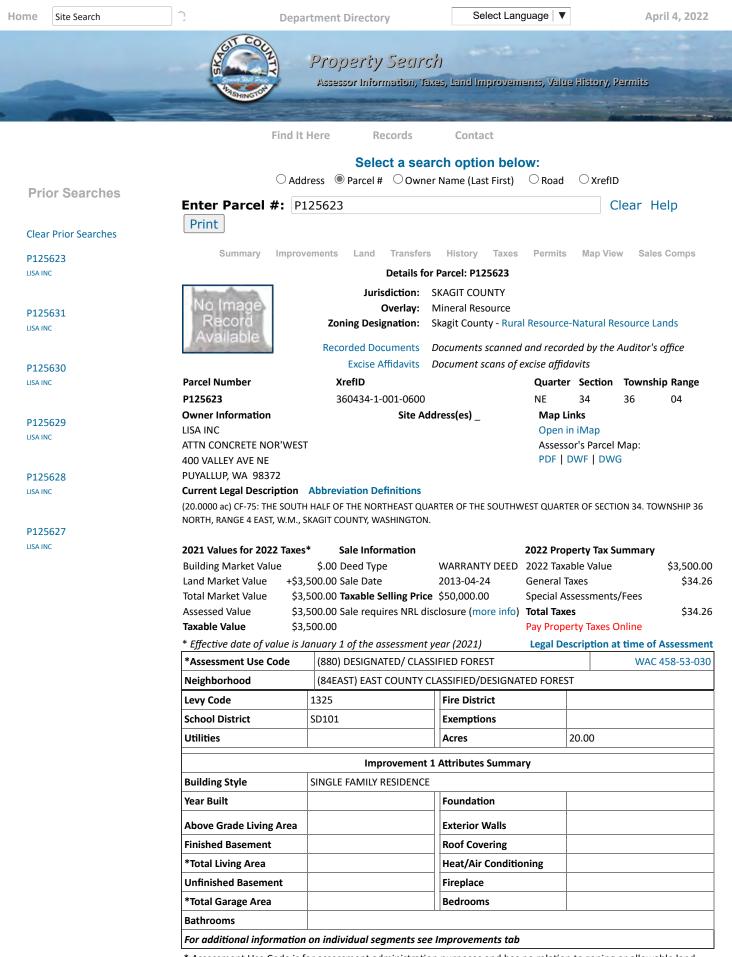
* Assessment Use Code is for assessment administration purposes and has no relation to zoning or allowable land use.

* Total living area includes above grade living area and finished basement area.

* Garage square footage includes all garage areas; basement garages, attached garages, detached garages, etc.

Assessment data for improvements is based on exterior inspections. Please contact the Assessor's office if the information does not accurately reflect the interior characteristics.

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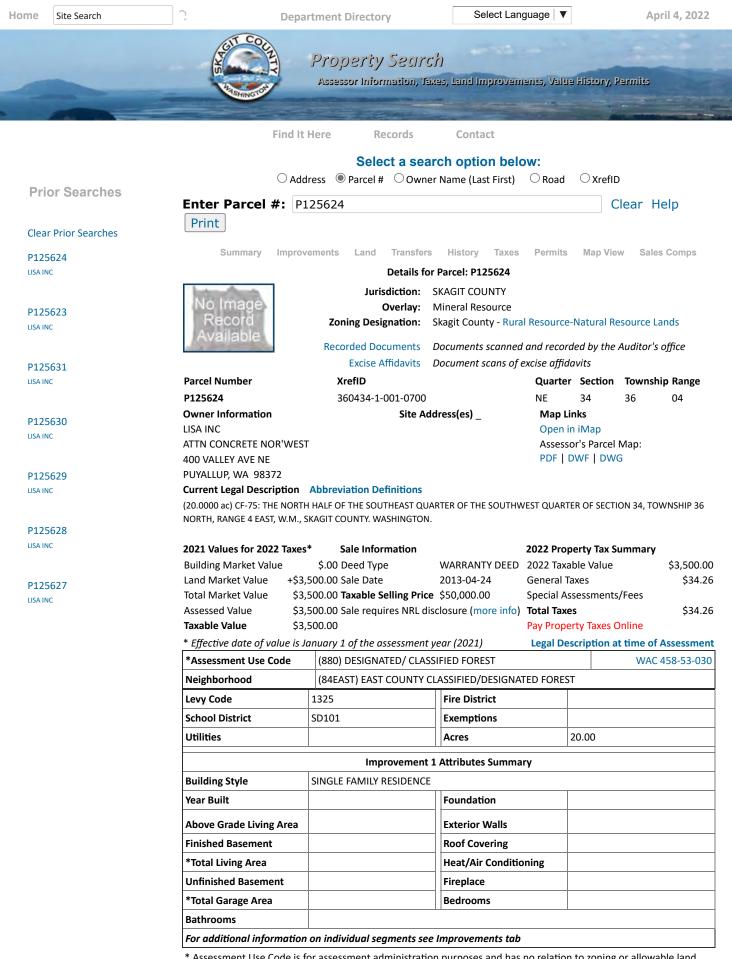
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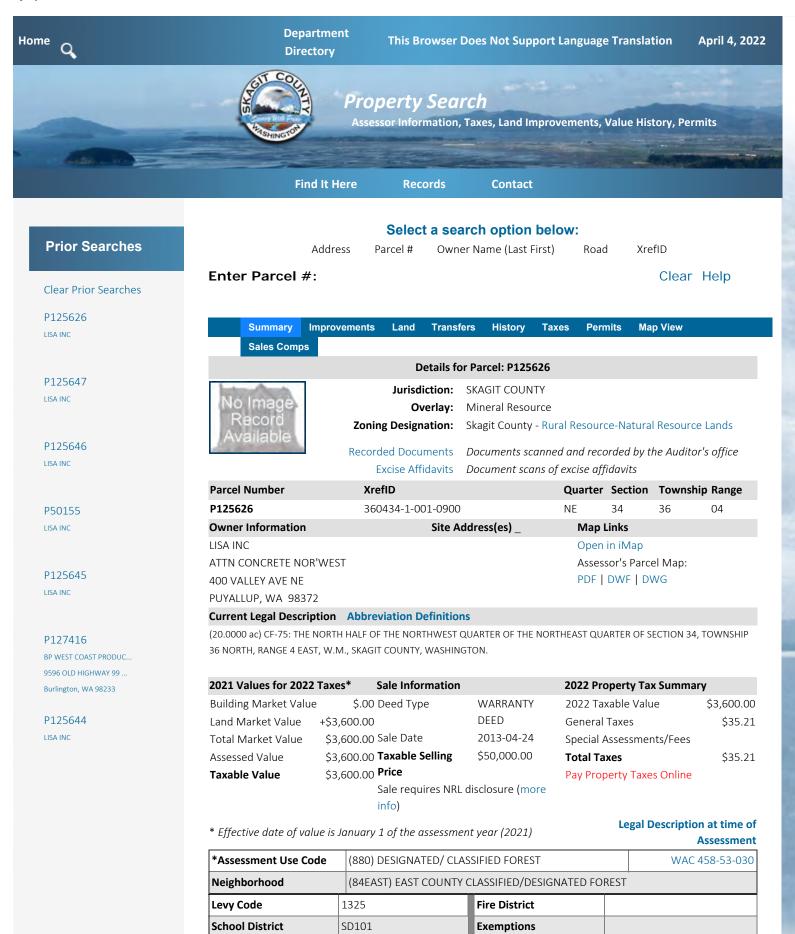
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Тор

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Property Search



Acres

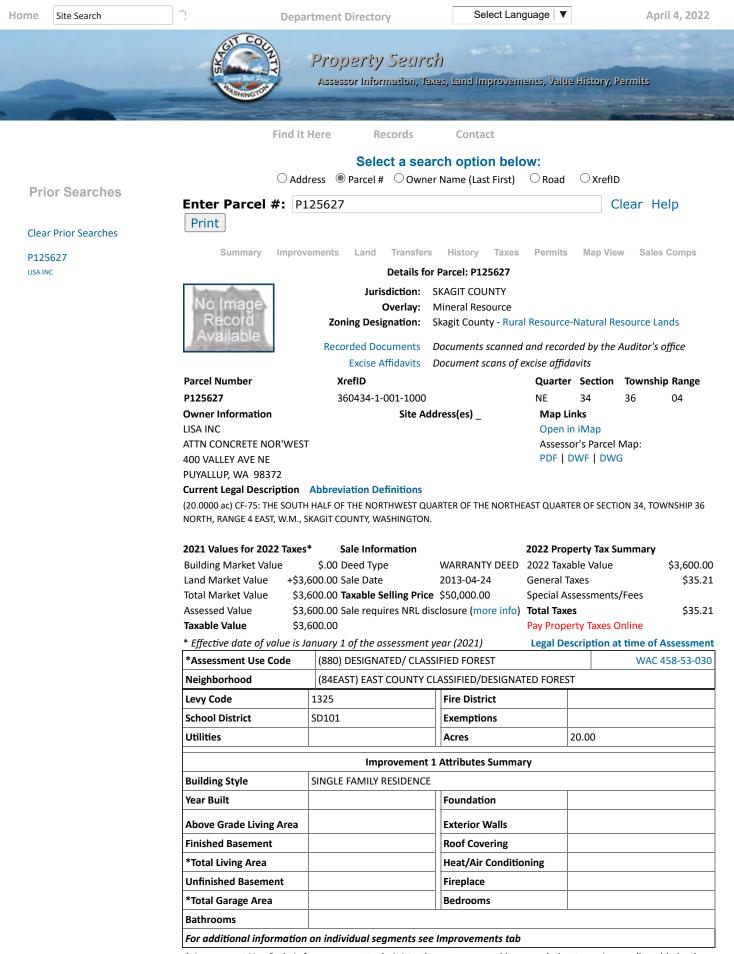
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Utilities

SINGLE FAMILY RESIDENCE		
Foundation Exterior Walls		
	Roof Covering	
Heat/Air Conditioning Fireplace		
	Bedrooms	
		Foundation Exterior Walls Roof Covering Heat/Air Conditioning Fireplace

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Assessment data for improvements is based on exterior inspections. Please contact the Assessor's office if Back the information does not accurately reflect the interior characteristics.



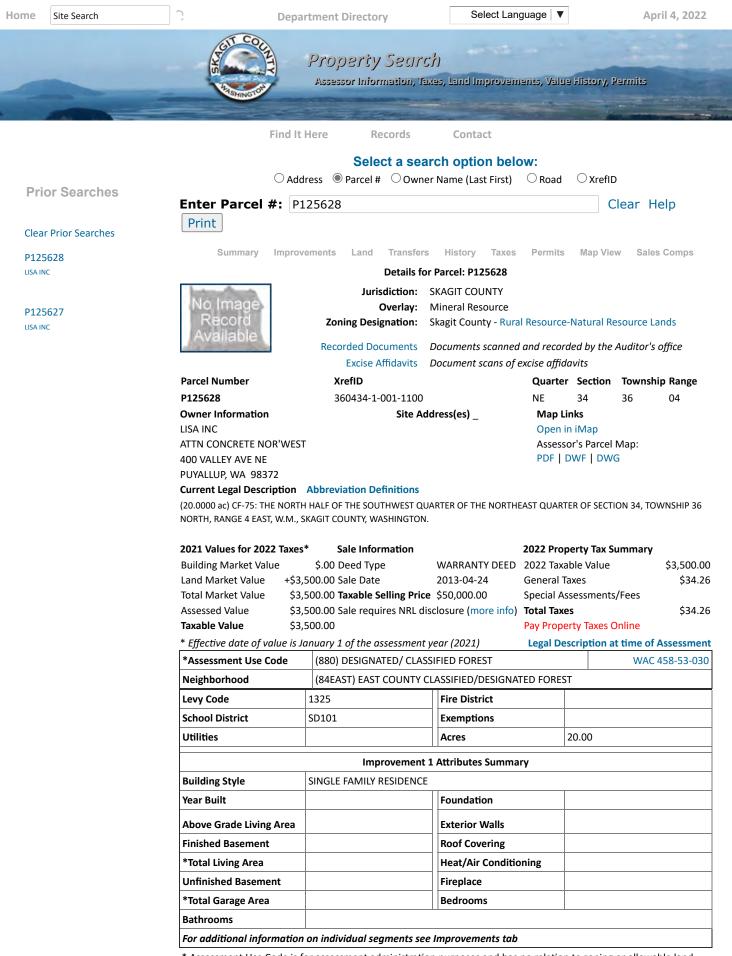
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Assessment data for improvements is based on exterior inspections. Please contact the Assessor's office if the information does not accurately reflect the interior characteristics.

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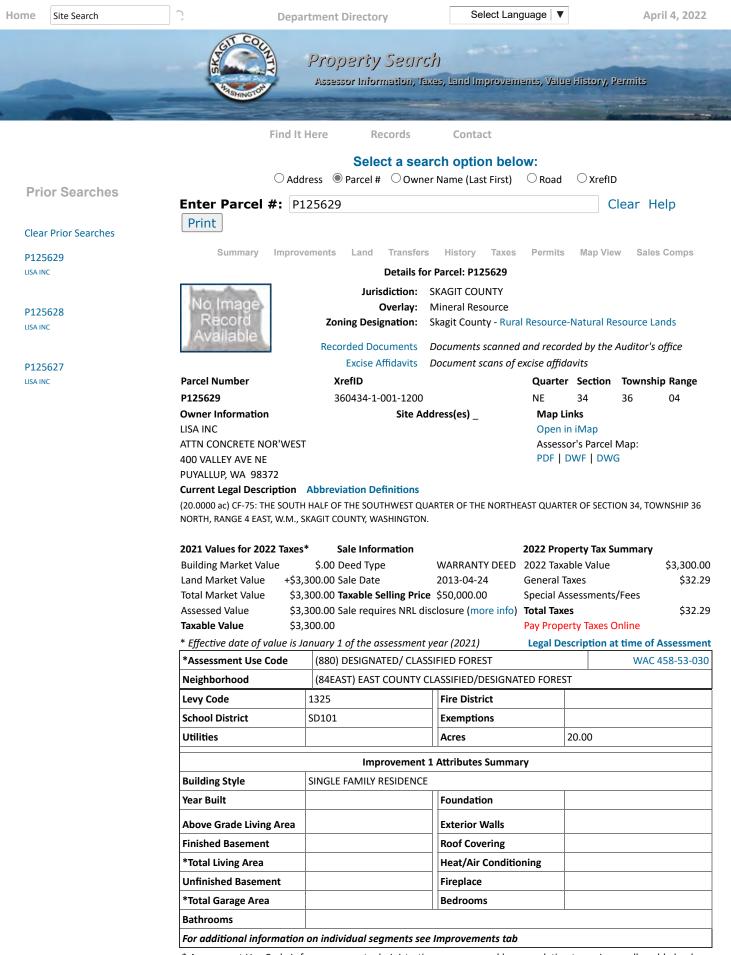
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Assessment data for improvements is based on exterior inspections. Please contact the Assessor's office if the information does not accurately reflect the interior characteristics.

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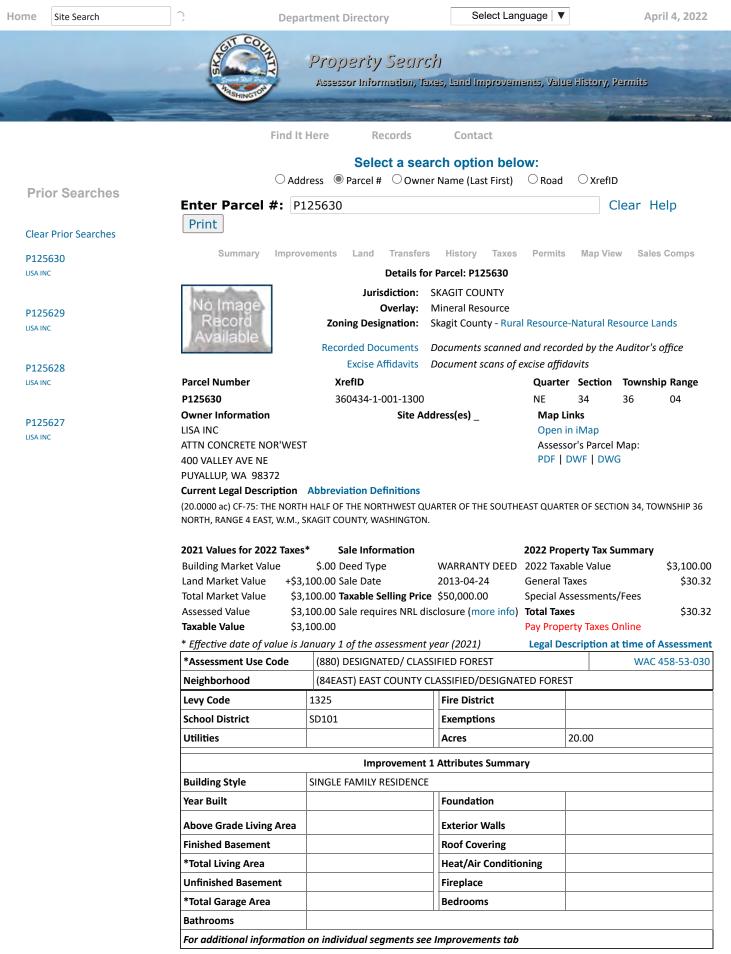
© 2021 Skagit County



^{*} Assessment Use Code is for assessment administration purposes and has no relation to zoning or allowable land use.

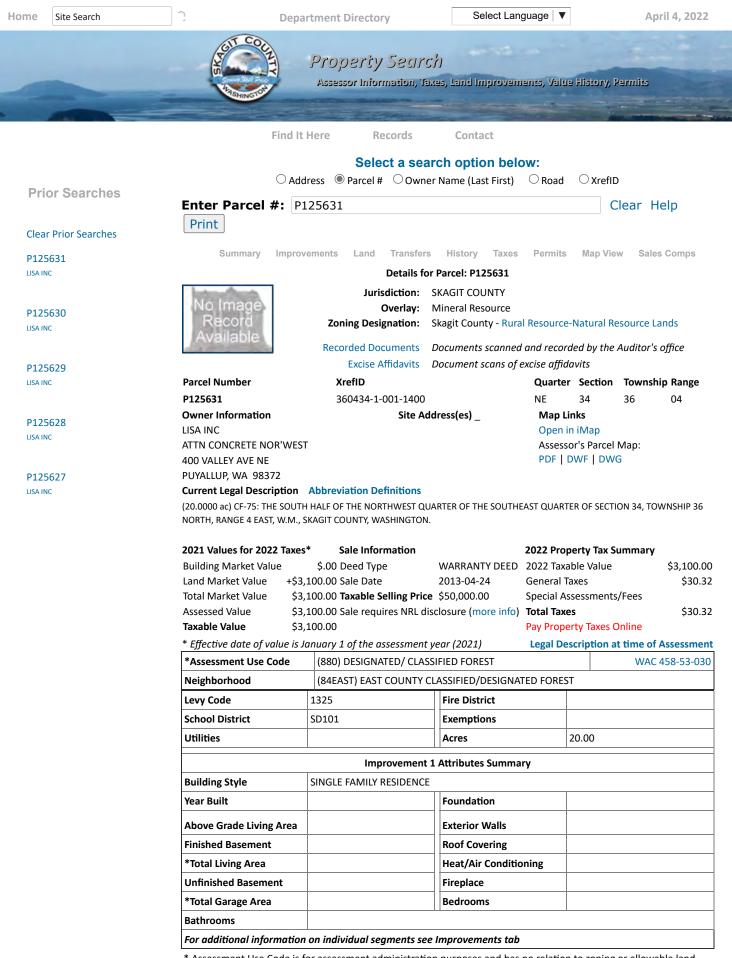
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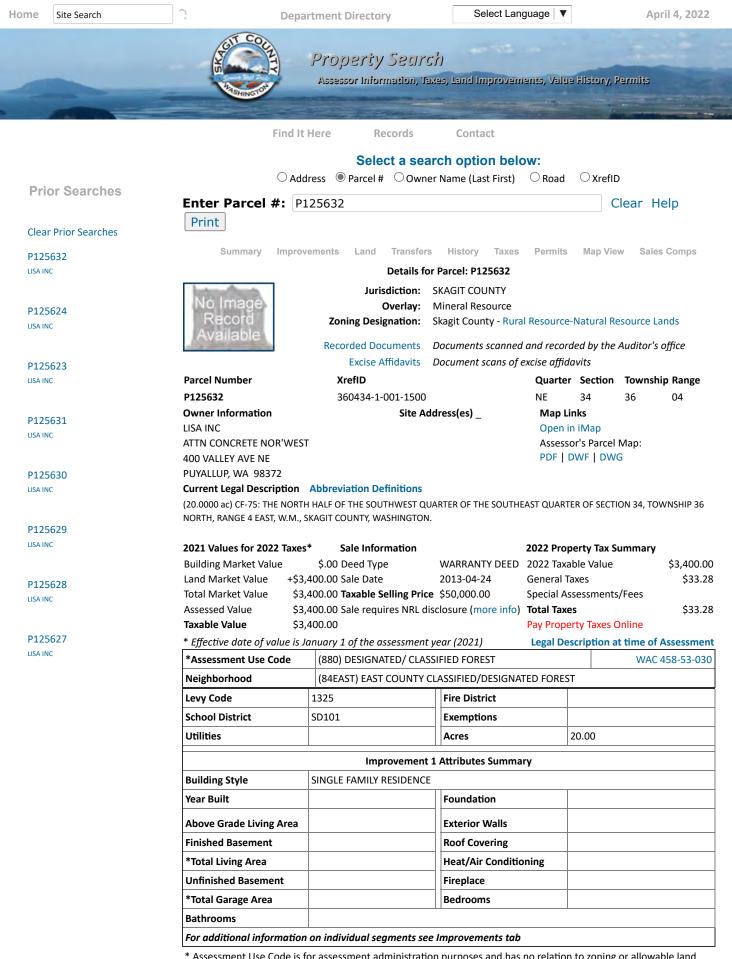
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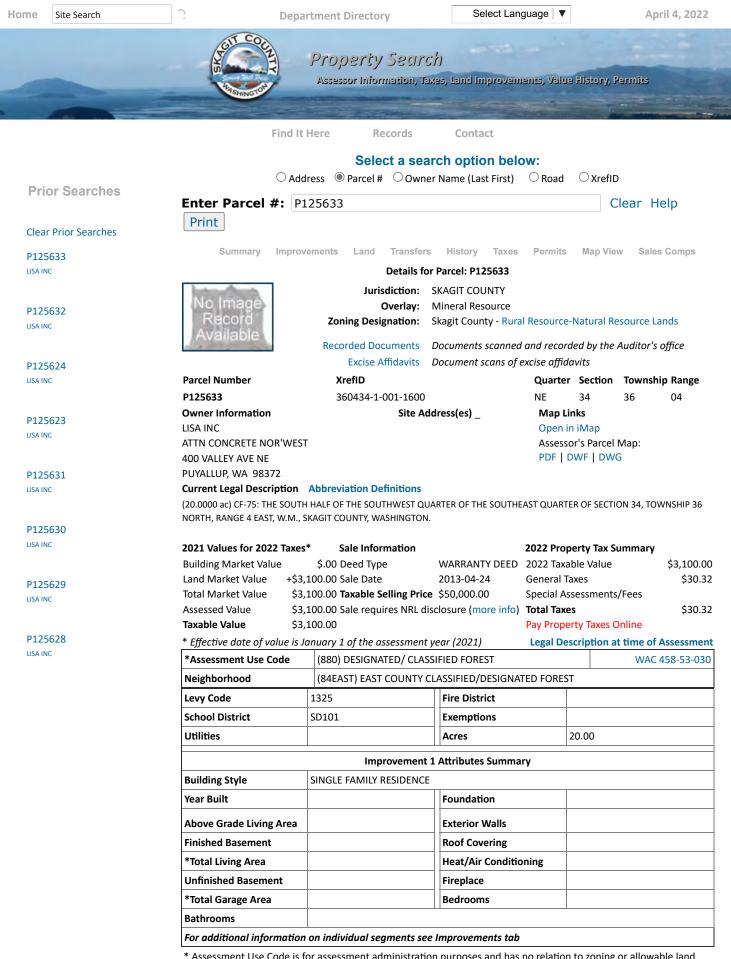
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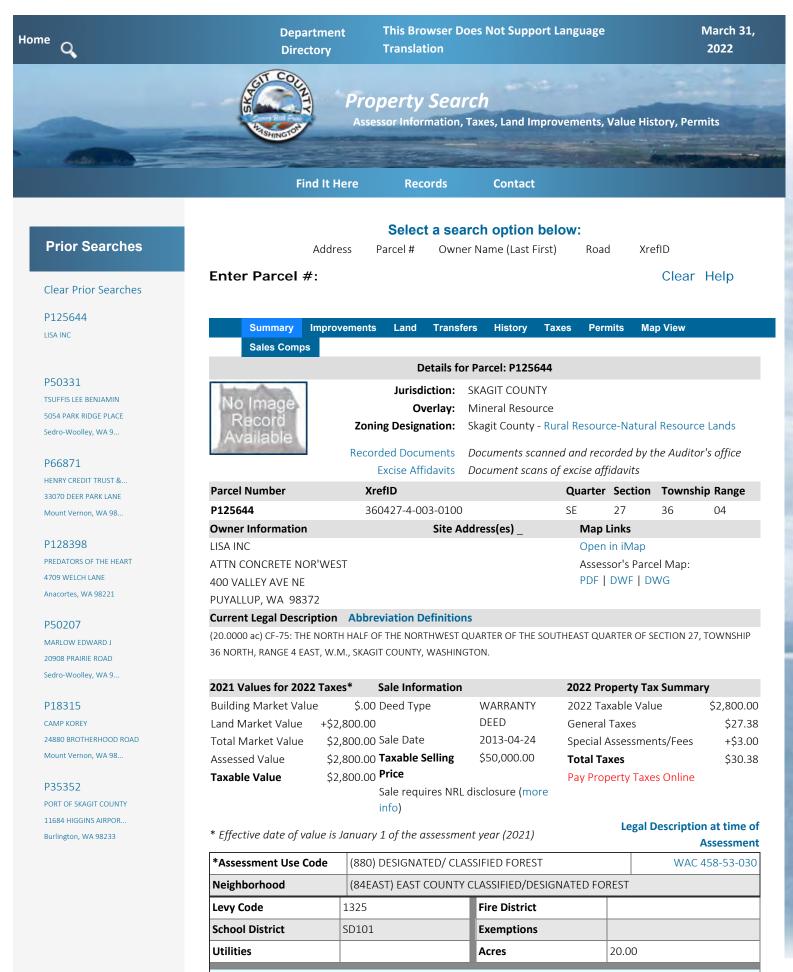


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Assessment data for improvements is based on exterior inspections. Please contact the Assessor's office if the Bac information does not accurately reflect the interior characteristics.

Property Search



Improvement 1 Attributes Summary

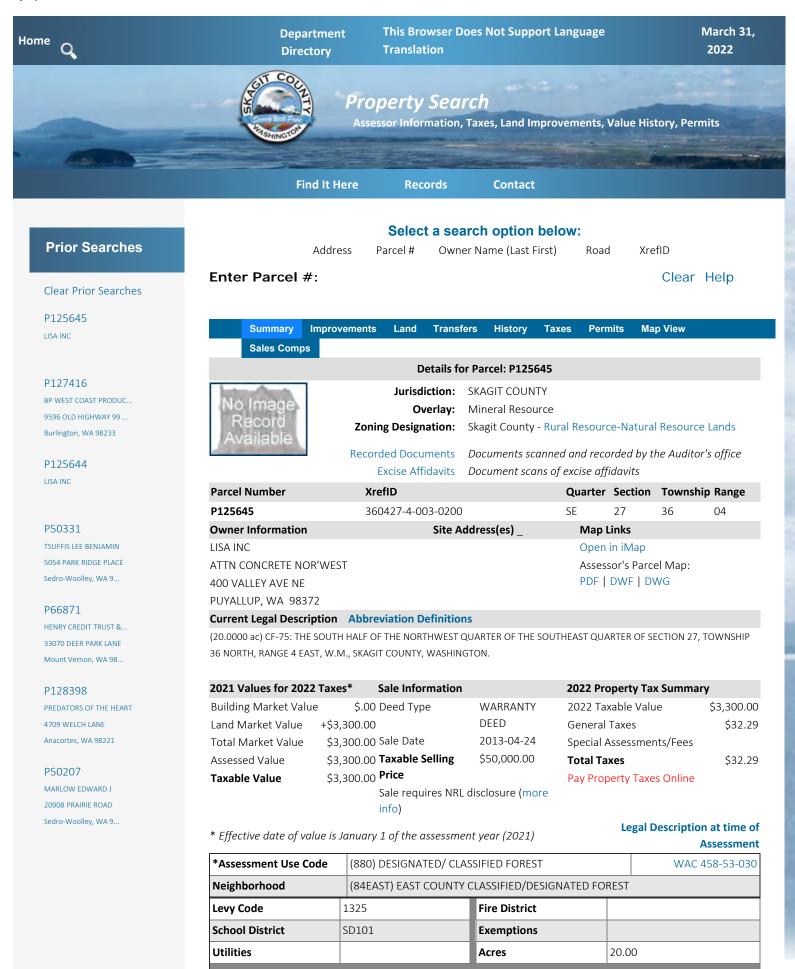
Building Style	SINGLE FAMILY RESIDENCE			
Year Built		Foundation		
Above Grade Living Area		Exterior Walls		
Finished Basement	Roof Covering Heat/Air Conditioning Fireplace			
*Total Living Area				
Unfinished Basement				
*Total Garage Area		Bedrooms		
Bathrooms				
For additional informatio	on on individual segments s	ee Improvements tab		

* Total living area includes above grade living area and finished basement area.

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Assessment data for improvements is based on exterior inspections. Please contact the Assessor's office if Back the information does not accurately reflect the interior characteristics.

Property Search



Improvement 1 Attributes Summary

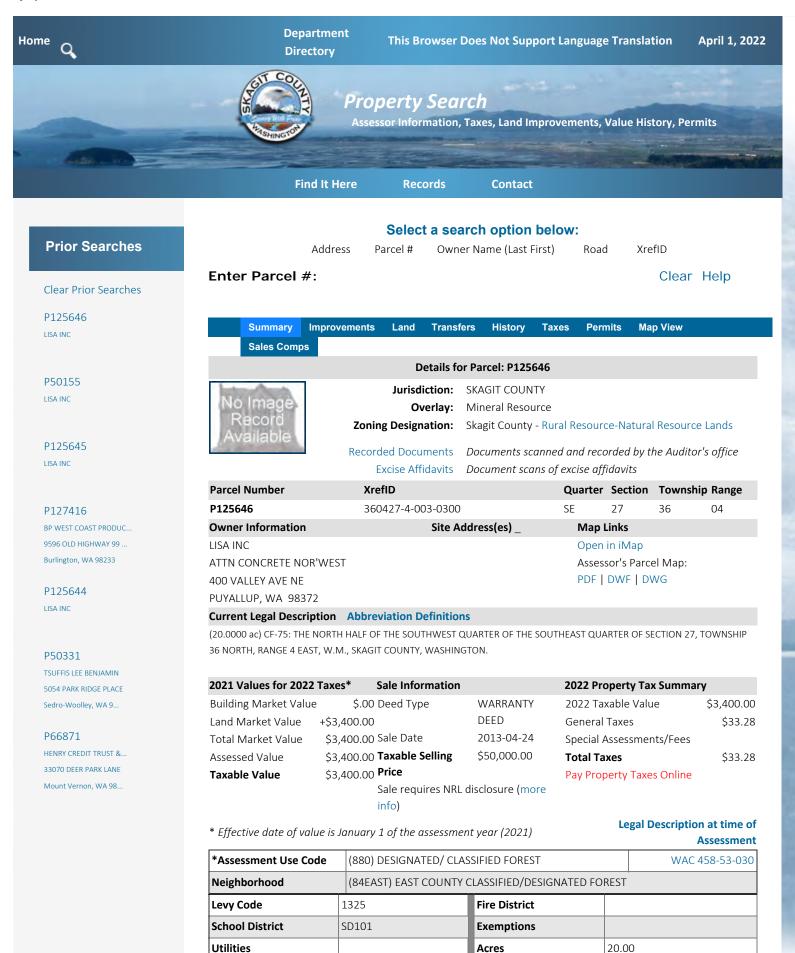
Building Style	SINGLE FAMILY RESIDENCE		
Year Built	Foundation		
Above Grade Living Area		Exterior Walls	
Finished Basement	Roof Covering Heat/Air Conditioning Fireplace		
*Total Living Area			
Unfinished Basement			
*Total Garage Area		Bedrooms	
Bathrooms		imi 1	1
For additional informatio	n on individual segments	see Improvements tab	

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Assessment data for improvements is based on exterior inspections. Please contact the Assessor's office if Back the information does not accurately reflect the interior characteristics.

Property Search



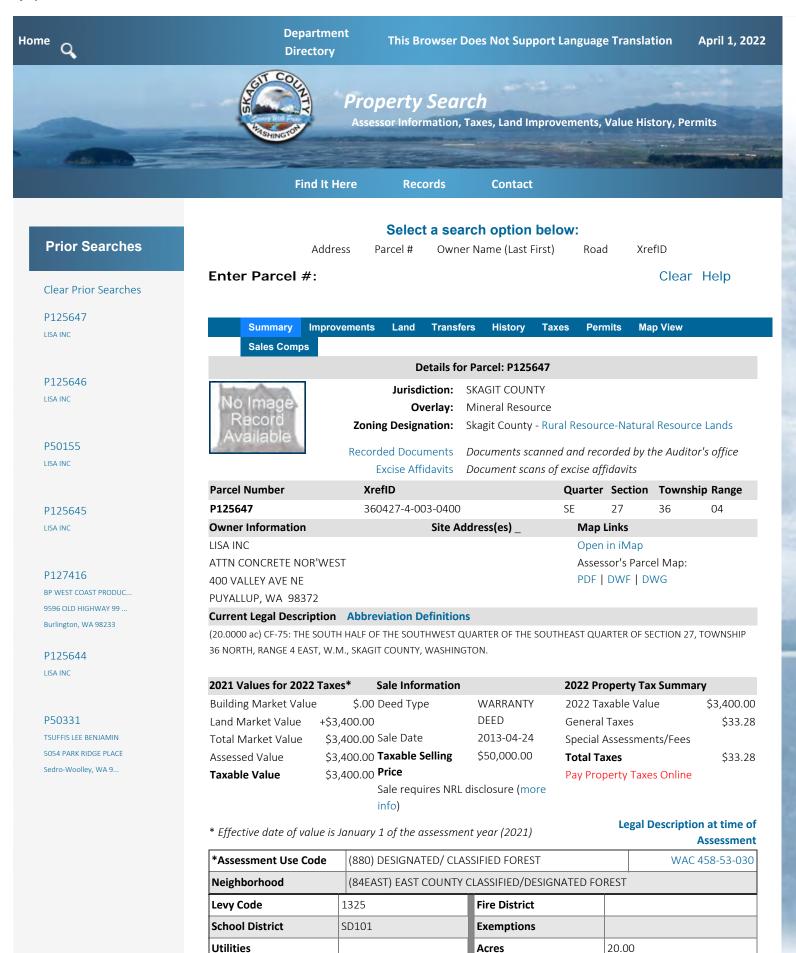
Improvement 1 Attributes Summary

Building Style	SINGLE FAMILY RESIDENCE		
Year Built	Foundation		
Above Grade Living Area		Exterior Walls	
Finished Basement		Roof Covering	
*Total Living Area		Heat/Air Conditioning	
Unfinished Basement		Fireplace	
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Bathrooms			
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Property Search



Improvement 1 Attributes Summary

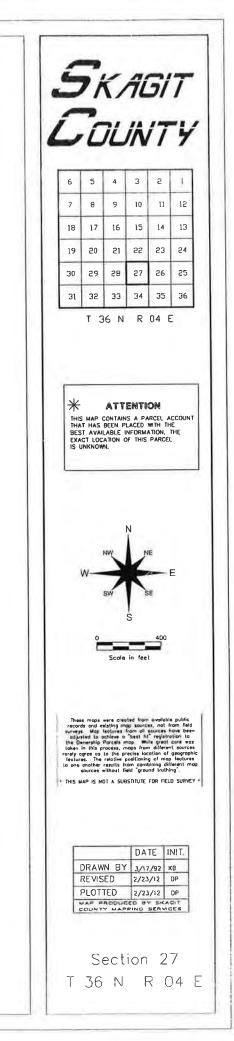
SINGLE FAMILY RESIDENCE		
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'n		

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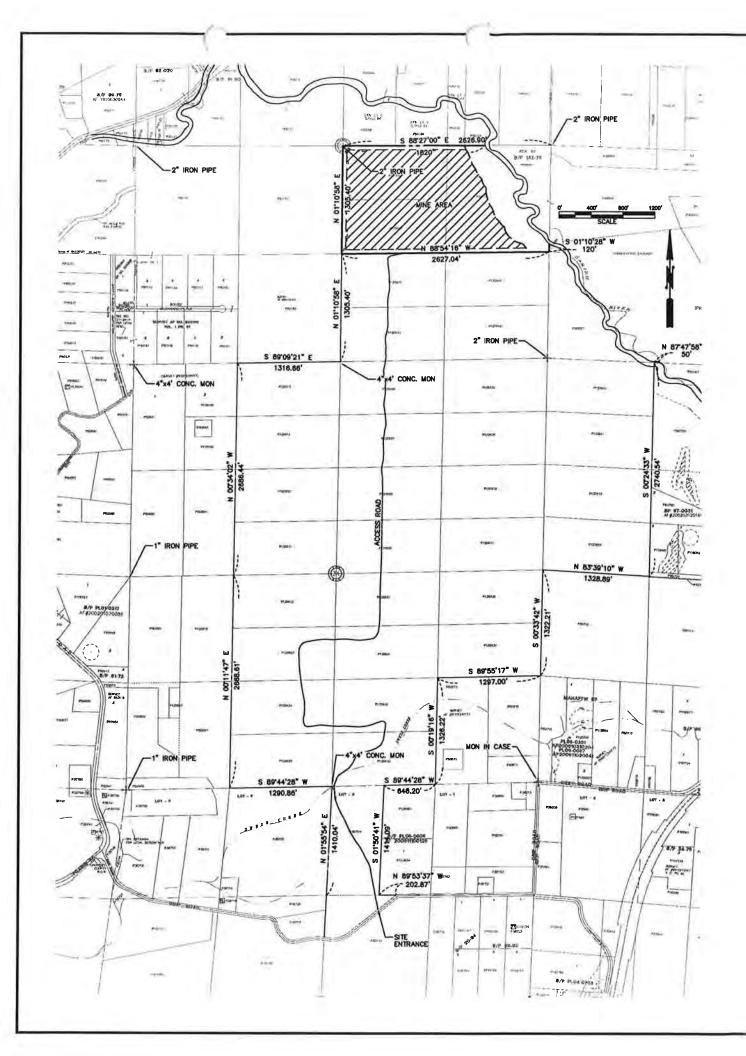




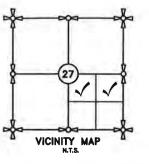


PLIG- 6097 RECEIVED

N 1 7 2016 SKAGIT COUNTY PDS



GRIP ROAD GRAVEL MINE GRIP ROAD, SEDRO-WOOLLEY WA IN A PORTION OF N 1/2 OF SE 1/4 OF SECTION 27, TOWNSHIP 36 NORTH, RANGE 4 EAST, W.M. SKAGIT COUNTY, WASHINGTON





APPLICANT:	CONCRETE NOR'WEST
SITE ENTRANCE ADDRESS:	GRIPP ROAD SEDRO-WOOLLEY, WA 98284
ZONING JURISDICTION: CURRENT ZONING:	SKAGIT COUNTY RURAL RESOURCE / NATURAL RESOURCE
COMPREHENSIVE PLAN:	RURAL RESOURCE / NATURAL RESOURCE
MINE AREA: RECLAMATION AREA:	68.8 ACRES 50.8 ACRES

SURVEY DESCRIPTION

P50155 -

THE NORTHEAST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 27, TOWNSHIP 36 NORTH, RANGE 4 EAST, W.M., SKAGIT COUNTY, WASHINGTON:

EXCEPT THAT PORTION LYING NORTHEASTERLY OF THE SAMISH RIVER AS IT PRESENTLY EXISTS; SUBJECT TO AND TOGETHER WITH AND EASEMENT FOR INGRESS, EGRESS, AND UTILITIES OVER TH EAST 30 FEET OF THE EAST HALF OF THE SOUTHEAST QUARTER OF SECTION 27, TOWNSHIP 35 NORTH, RANGE 4 EAST, W.M., SKAGIT COUNTY, WASHINGTON, LYING SOUTH OF THE SAMISH RIVER

EXCEPT THAT PORTION LYING NORTHEASTERLY OF THE SAMISH RIVER AS IT PRESENTLY EXISTS;

SUBJECT TO EASEMENTS AND RESTRICTIONS OF RECORD, IF ANY.

<u> P125644</u> -

THE NORTH HALF OF THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 27, TOWNSHIP 36 NORTH, RANGE 4 EAST, W.M., SKAGIT COUNTY, WASHINGTON:

SUBJECT TO AND TOGETHER WITH AND EASEMENT FOR INGRESS, EGRESS, AND UTILITIES OVER THE WEST 30 FEET OF THE WEST HALF OF THE SOUTHEAST QUARTER OF SECTION 27, TOWNSHIP 36 NORTH, RANGE 4 EAST, W.M., SKAGIT COUNTY, WASHINGTON;

SUBJECT TO EASEMENTS AND RESTRICTIONS OF RECORD, IF ANY.

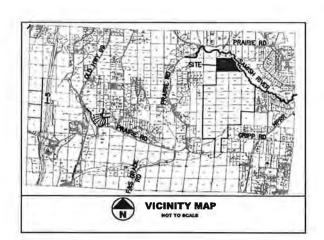
<u> P125845</u> -

THE SOUTH HALF OF THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 27, TOWNSHIP 36 NORTH, RANGE 4 EAST, W.M., SKAGIT COUNTY, WASHINGTON:

SUBJECT TO AND TOGETHER WITH AND EASEMENT FOR INGRESS, EGRESS, AND UTILITIES OVER THE WEST JO FEET OF THE WEST HALF OF THE SOUTHEAST QUARTER OF SECTION 27, TOWNSHIP 36 NORTH, RANGE 4 EAST, W.M., SKAGIT COUNTY, WASHINGTON;

SUBJECT TO EASEMENTS AND RESTRICTIONS OF RECORD, IF ANY.





APPLICANT/OWNER

CONCRETE NOR'WEST P.O. BOX 280 MOUNT VERNON, WA 98273 CONTACT: DAN COX TEL: (360) 757-3121

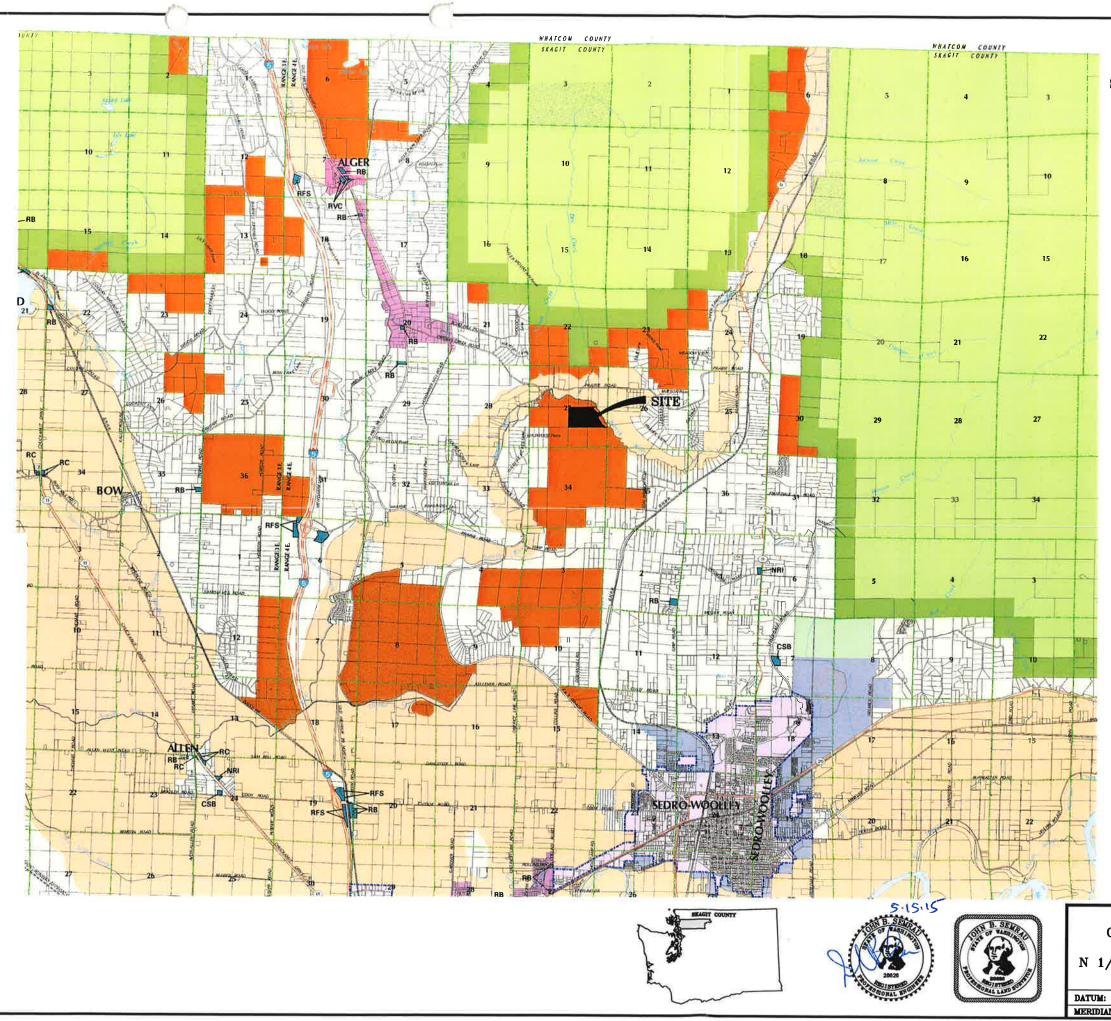
CIVIL ENGINEER/SURVEYOR SEMRAU ENGINEERING AND SURVEYING 2118 RIVERSIDE DRIVE SUITE 208 MOUNT VERNON, WA 98273 CONTACT: JOHN B. SEMRAU P.E. & P.L.S. TEL: (360) 424-9566

GEOTECHNICAL/GEOLOGICAL ENGINEER ASSOCIATED EARTH SCIENCES 2911 1/2 HEWITT AVENUE SUITE 2 EVERETT, WA 98201 CONTACT: CHUCK LINDSAY P.G., P.E.G. & P.Hg. TEL: (425) 259-0522

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CO	VICINITY MAP
CI	CONTENTS AND VICINITY MAP
C2	PROPERTY OWNERSHIP MAP
	PRE-MINING TOPOGRAPHIC SURVEY MAP
	PROPOSED MINE CONTOURS
	RECLAMATION PLAN AND MINE SEQUENCE
C6	CROSS SECTIONS
	C1 C2 C3 C4 C5

	SHI	ET C1 OF 7 PL	16-0000
	IP ROAD GF GR OF SE 1/4	NTENTS AND VICINITY AVEL MINE PERMIT IP ROAD, SEDRO-WOO OF SECTION 27, T. AGIT COUNTY, WASHIN	Γ NO. 16-XXXXXX DLLEY 36 N., R. 4 E., W.M.
: AN:	NAVD 88 WSPN	SEMRAU ENGINEERING & SUI SURVEYING - ENGINEERING - PLAN MOUNT VERNON, WA 98273 360-42	NING



GRIP ROAD GRAVEL MINE GRIP ROAD, SEDRO-WOOLLEY WA IN A PORTION OF N 1/2 OF SE 1/4 OF SECTION 27, TOWNSHIP 36 NORTH, RANGE 4 EAST, W.M. SKAGIT COUNTY, WASHINGTON LEGEND Incorporated Areas [UGA] Urban Growth Areas RURAL [RRv] Rural Reserve [RI] Rural Intermediate [RV] Rural Village Residential NATURAL RESOURCE LAND [NRL] [RRc-NRL] Rural Resource - NRL [Ag-NRL] Agriculture - NRL [SF-NRL] Secondary Forest - NRL [IF-NRL] Industrial Forest - NRL [MRO] Mineral Resource Overlay COMMERCIAL / INDUSTRIAL Commercial /Industrial Rural Business Rural Center 3 Rural Village Commercial 3 Rural Freeway Service 1 Natural Resource Industrial 3 Small-Scale Recreation & Tourism 3 Cottage Industry / Small-Scale Business Rural Marine Industrial 4 Major Industrial Developments 1 Nonzonetties o urrently designa bach [RB] [RC] [RVC] [RFS] [NRI] [SRT] [SRT] [CSB] [RMI] [MD] (No properties currently designated) Master Planned Resorts (No properties currently designated) [MPR] OPEN SPACE [OSRSI] Public Open Space of Statewide/Regional Importance SURVEY DESCRIPTION P50155 -THE NORTHEAST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 27, TOWNSHIP 36 NORTH, RANGE 4 EAST, W.M., SKAGIT COUNTY, WASHINGTON: EXCEPT THAT PORTION LYING NORTHEASTERLY OF THE SAMISH RIVER AS IT PRESENTLY EXISTS; SUBJECT TO AND TOGETHER WITH AND EASEMENT FOR INGRESS, EGRESS, AND UTILITIES OVER THE EAST 30 FEET OF THE EAST HALF OF THE SOUTHEAST QUARTER OF SECTION 27, TOWNSHIP 36 NORTH, RANGE 4 EAST, W.M., SKAGIT COUNTY, WASHINGTON, LYING SOUTH OF THE SAMISH RIVER; EXCEPT THAT PORTION LYING NORTHEASTERLY OF THE SAMISH RIVER AS IT PRESENTLY EXISTS; SUBJECT TO EASEMENTS AND RESTRICTIONS OF RECORD, IF ANY. P125644 -THE NORTH HALF OF THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 27, TOWNSHIP 36 NORTH, RANGE 4 EAST, W.M., SKAGIT COUNTY, WASHINGTON: SUBJECT TO AND TOGETHER WITH AND EASEMENT FOR INGRESS, EGRESS, AND UTILITIES OVER THE WEST 30 FEET OF THE WEST HALF OF THE SOUTHEAST QUARTER OF SECTION 27, TOWNSHIP 36 NORTH, RANGE 4 EAST, W.M., SKAGIT COUNTY, WASHINGTON;

SUBJECT TO EASEMENTS AND RESTRICTIONS OF RECORD, IF ANY.

<u> P125645</u> -

THE SOUTH HALF OF THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 27, TOWNSHIP 36 NORTH, RANGE 4 EAST, W.M., SKAGIT COUNTY, WASHINGTON:

SUBJECT TO AND TOGETHER WITH AND EASEMENT FOR INGRESS, EGRESS, AND UTILITIES OVER THE WEST 30 FEET OF THE WEST HALF OF THE SOUTHEAST QUARTER OF SECTION 27, TOWNSHIP 36 NORTH, RANGE 4 EAST, W.M., SKAGIT COUNTY, WASHINGTON;

SUBJECT TO EASEMENTS AND RESTRICTIONS OF RECORD, IF ANY.

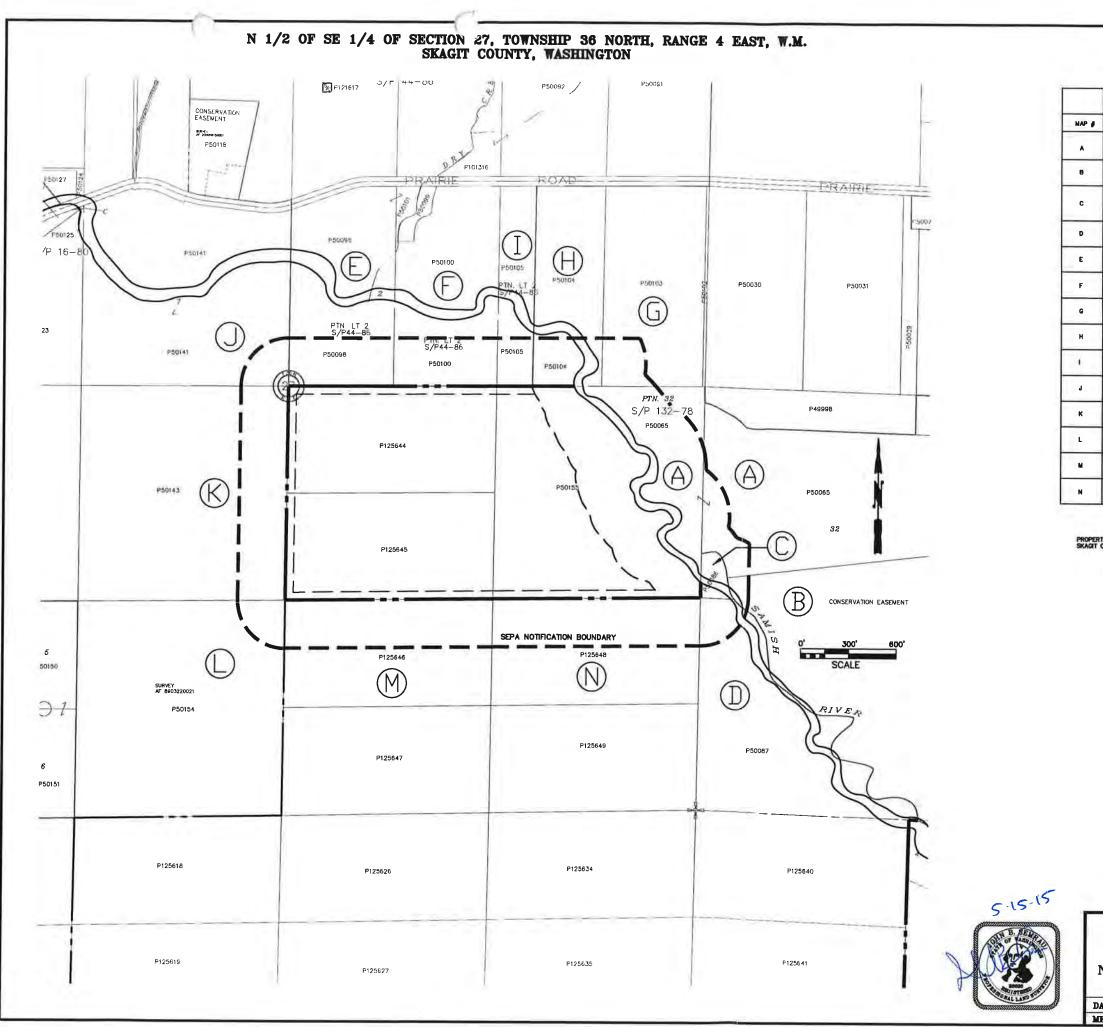
SHEET CO OF 7 PL15-0000

VICINITY MAP

GRIP ROAD GRAVEL MINE PERMIT NO. 15-XXXXXX GRIP ROAD, SEDRO-WOOLLEY

N 1/2 OF SE 1/4 OF SECTION 27, T. 36 N., R. 4 E., W.M. SKAGIT COUNTY, WASHINGTON

(:	NAVD 88	SEMRAU ENGINEERING & SURVEYING SURVEYING • ENGINEERING • PLANNING	SCALE:	1"= 600'	
IAN:	WSPN	MOUNT VERNON, WA 98273 360-424-9566	JOB NO	. 5166	



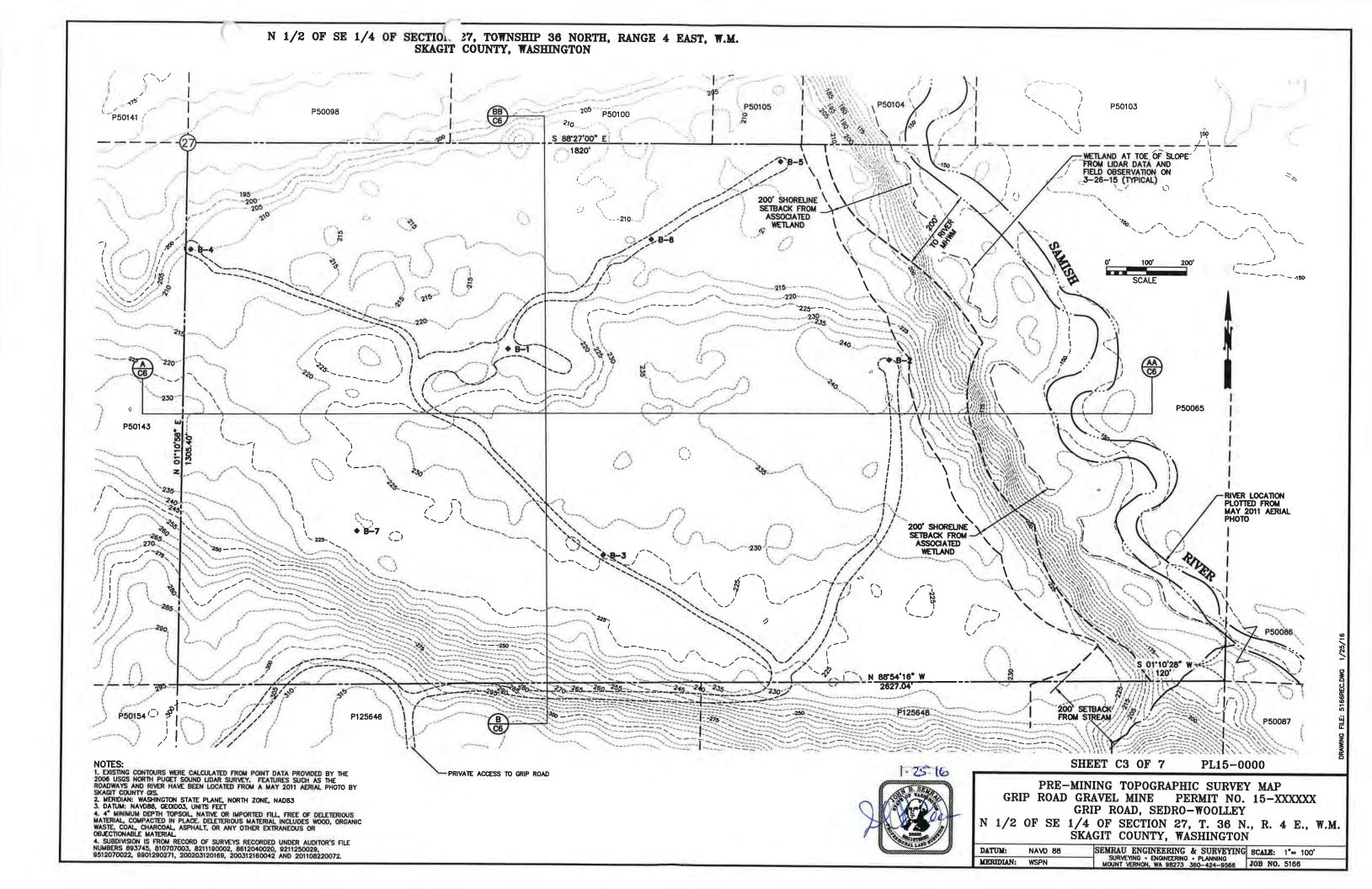
н P501 P501 1 J P5014 P501 к P5010 L P1256 M P1256 N

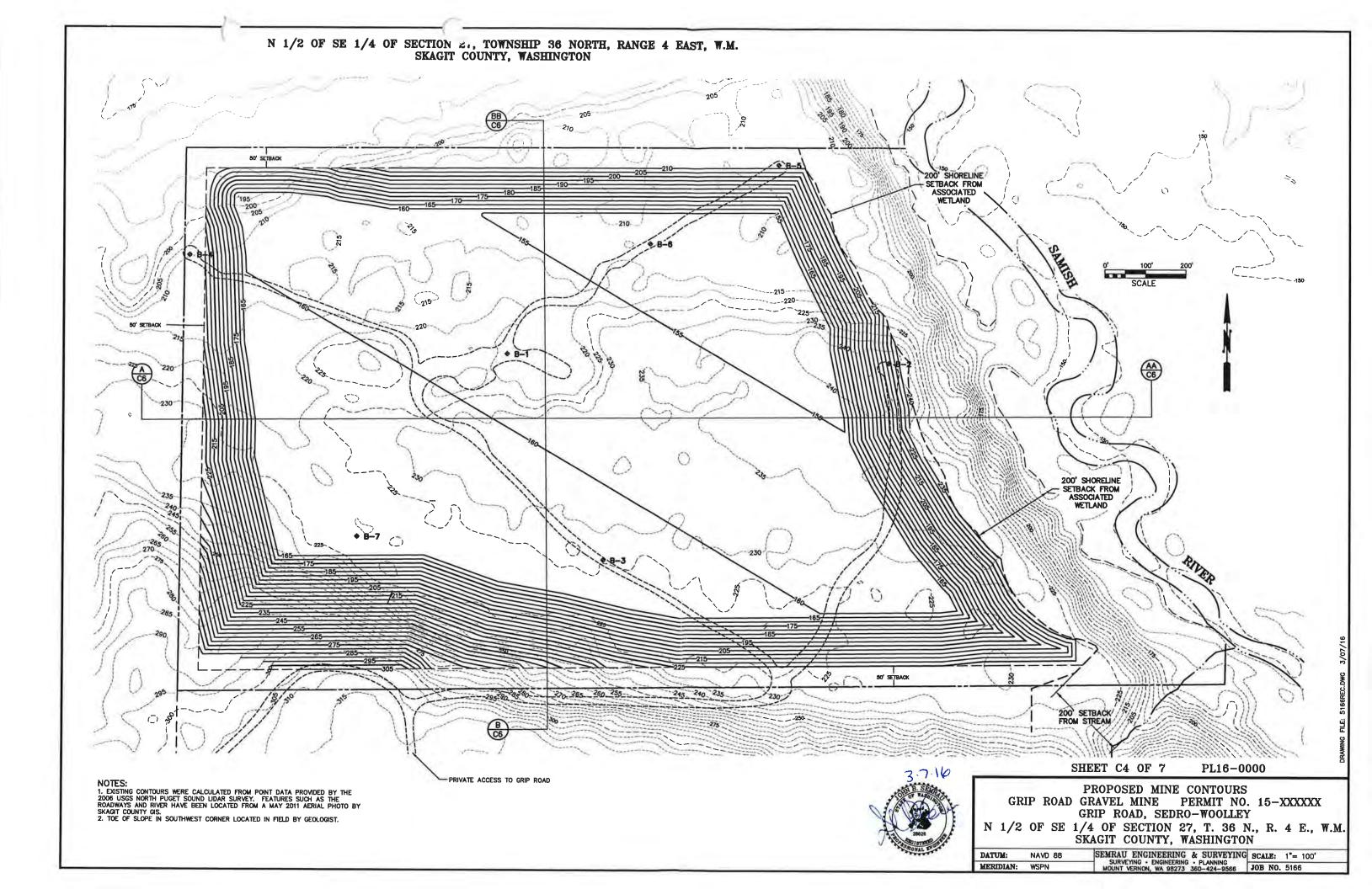
	PROPERTY OWNERSHIP TABLE	
PARCEL	OWNER	OWNER ADDRESS
P50065	JOHN AND JEANNE SHEA	P.O. BOX 311 BOW, WA. 98232
P50067	JOHN AND JEANNE SHEA	P.O. BOX 311 BOW, WA. 98232
P50086	PAUL KOETJE/C EMERSON AND GLORIA J CAMP/MARYANN FORD TAMARA J CONRAD	23318 127TH AVE NE ARLINGTON, WA 98223
P50087	LISA, INC. ATTN CONCRETE NOR'WEST	P.O. BOX 280 MOUNT VERNON, WA. 98273
P50098	RICK GILES	21858 PRAIRIE RD. SEDRO-WOOLLEY, WA. 98284
P50100	ROBERT AND LINDA WALSH	21710 PRAIRIE RD. SEDRO-WOOLLEY, WA. 98284
P50103	LARRY AND BETH VANDERVEEN	21994 PRAIRIE RD. SEDRO-WOOLLEY, WA. 98284
P50104	LARRY AND BETH VANDERVEEN	21994 PRAIRIE RD. SEDRO-WOOLLEY, WA. 98284
P50105	ROBERT AND LINDA WALSH	21710 PRAIRIE RD. SEDRO-WOOLLEY, WA. 98284
P60141	DANIELLE AND JASON HAUGLAND	21422 PRAIRIE RD. SEDRO-WOOLLEY, WA. 98284
P50143	EDWARD AND ROBERTA ROBINSON	21000 PRARIE RD. SEDRO-WOOLLEY, WA. 98284
P50164	EDWARD AND ROBERTA ROBINSON	21000 PRAIRIE RD. SEDRO-WOOLLEY, WA. 98284
P125648	lisa, inc. Attn concrete nor'west	P.O. BOX 280 MOUNT VERNON, WA. 98273
125648	LISA, INC. ATTN CONCRETE NOR'WEST	P.O. BOX 280 MOUNT VERNON, WA. 98273

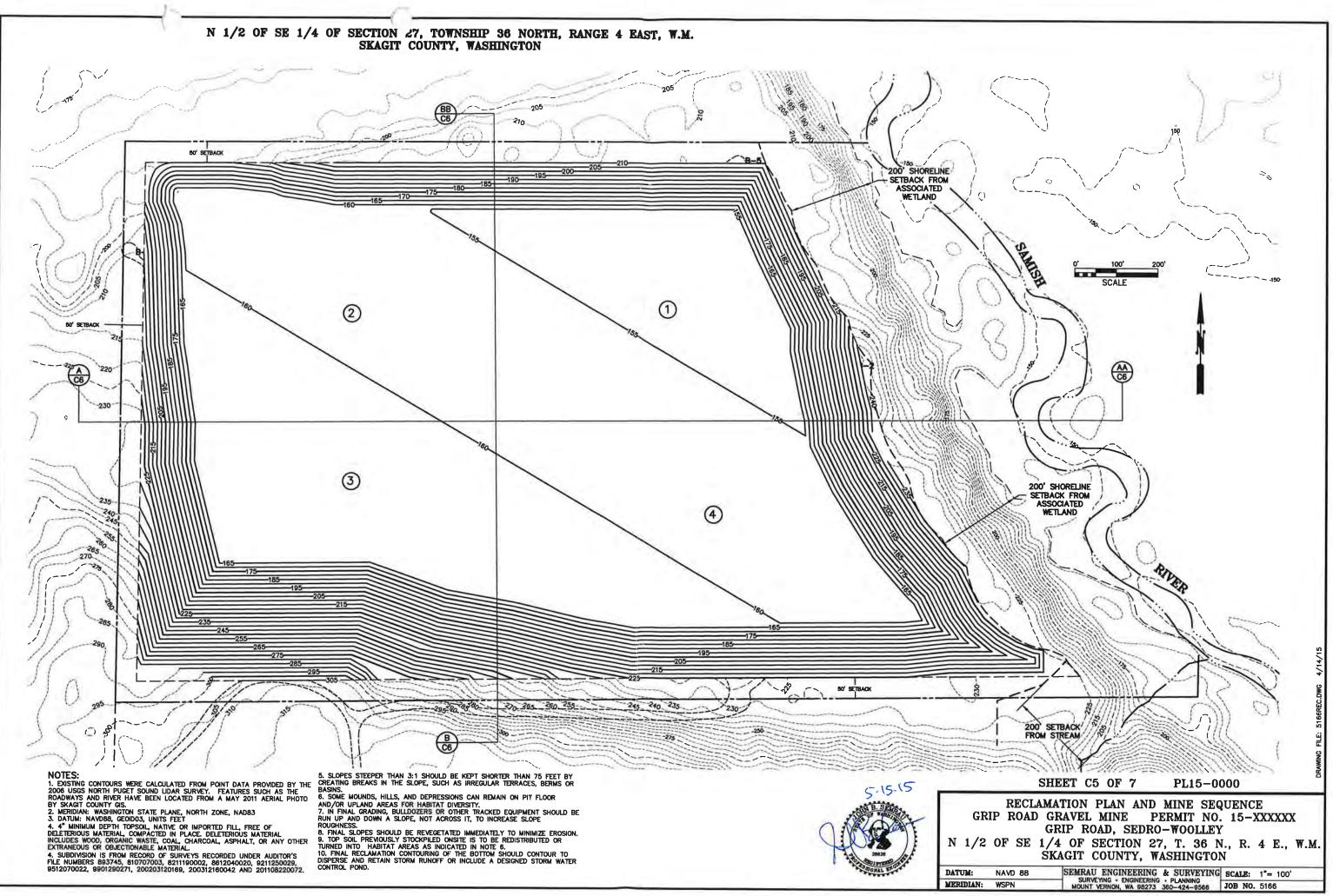
PROPERTY OWNERSHIP DATA AND MAPS WERE TAKEN FROM SKAGT COUNTY ASSESSOR'S ELECTRONIC DATA.

SHEET	C2	OF	7	PL15-0000

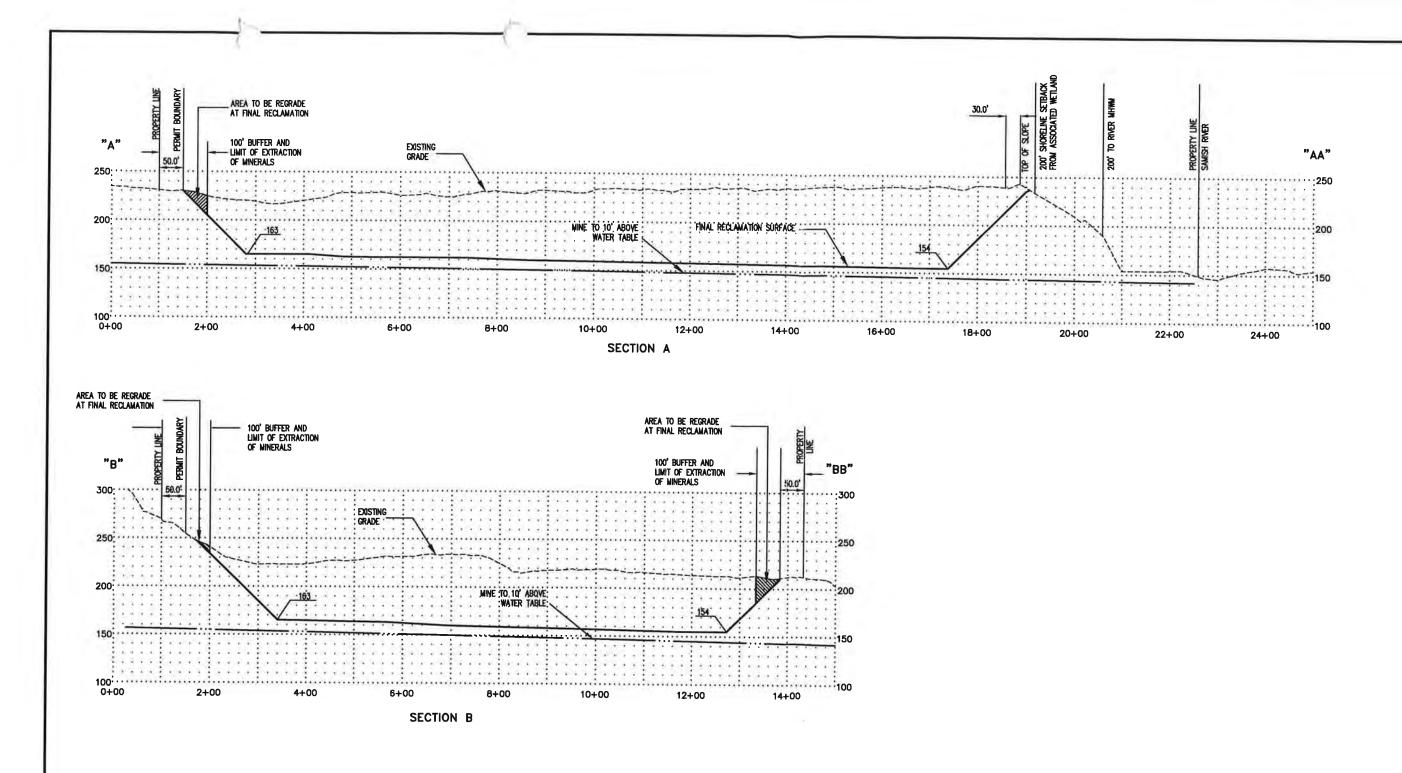
		PROPERTY OWNERSHIP MAP GRAVEL MINE PERMIT NO. 15-XXXXXX GRIP ROAD, SEDRO-WOOLLEY 1/4 OF SECTION 27, T. 36 N., R. 4 E., W.M. SKAGIT COUNTY, WASHINGTON
DATUM:	NAVD 88	SEMRAU ENGINEERING & SURVEYING SCALE: 1"= 300'
MERIDIAN:	WSPN	SURVEYING · ENGINEERING · PLANNING MOUNT VERNON, WA 98273 360-424-9566 JOB NO. 5166







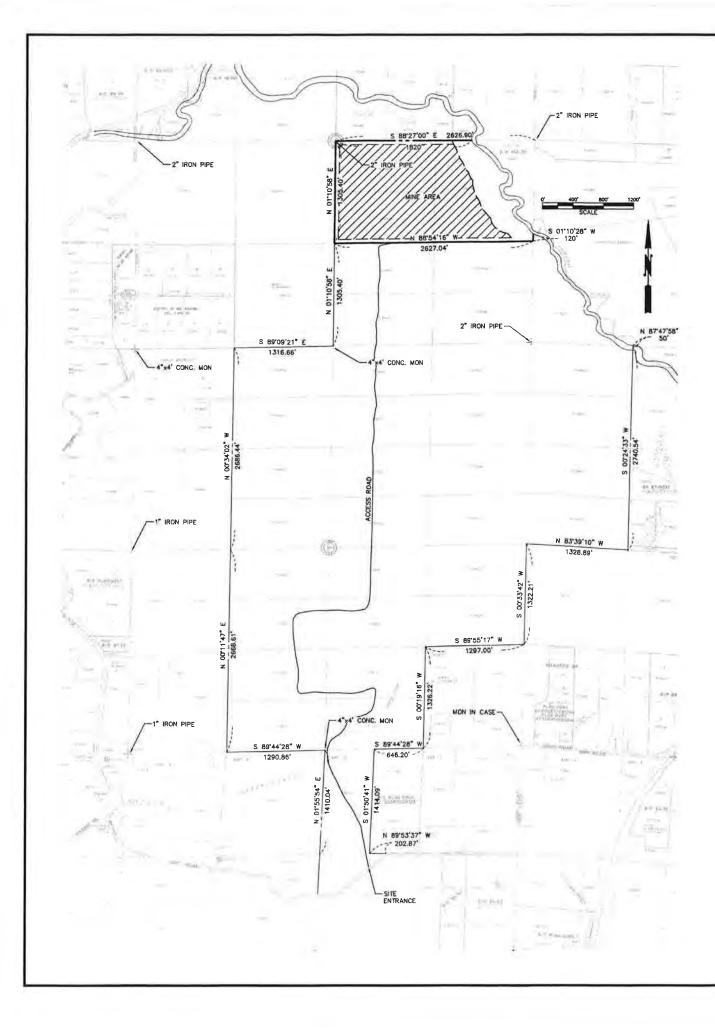




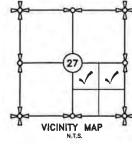


SHEET C6 OF 7 PL15-0000

GRIP	ROAI) GF	CROSS SECTIONS AVEL MINE PERMIT NO. 15-XXXXXX
		1/4	GRIP ROAD, BURLINGTON OF SECTION 27, T. 36 N., R. 4 E., W.M. AGIT COUNTY, WASHINGTON
TUM: RIDIAN:	NAVD WSPN	88	SEMRAU ENGINEERING & SURVEYING SCALE: 1"= 100' SURVEYING - ENGINEERING - PLANNING MOUNT VERNON, WA 98273 350-424-9566 JOB NO. 5166



GRIP ROAD GRAVEL MINE GRIP ROAD, SEDRO-WOOLLEY WA IN A PORTION OF N 1/2 OF SE 1/4 OF SECTION 27, TOWNSHIP 36 NORTH, RANGE 4 EAST, W.M. SKAGIT COUNTY, WASHINGTON





APPLICANT:	CONCRETE NOR'WEST
SITE ENTRANCE ADDRESS:	GRIPP ROAD SEDRO-WOOLLEY, WA 98284
ZONING JURISDICTION CURRENT ZONING:	SKAGIT COUNTY RURAL RESOURCE / NATURAL RESOURCE
COMPREHENSIVE PLAN:	RURAL RESOURCE / NATURAL RESOURCE
MINE AREA: RECLAMATION AREA:	68.8 ACRES 50.8 ACRES

SURVEY DESCRIPTION

<u> P50155</u> --

THE NORTHEAST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 27, TOWNSHIP 36 NORTH, RANGE 4 EAST, W.M., SKAGIT COUNTY, WASHINGTON:

EXCEPT THAT PORTION LYING NORTHEASTERLY OF THE SAMISH RIVER AS IT PRESENTLY EXISTS;

SUBJECT TO AND TOGETHER WITH AND EASEMENT FOR INGRESS, EGRESS, AND UTILITIES OVER THE EAST 30 FEET OF THE EAST HALF OF THE SOUTHEAST QUARTER OF SECTION 27, TOWNSHIP 36 NORTH, RANGE 4 EAST, W.M., SKAGIT COUNTY, WASHINGTON, LYING SOUTH OF THE SAMISH RIVER;

EXCEPT THAT PORTION LYING NORTHEASTERLY OF THE SAMISH RIVER AS IT PRESENTLY EXISTS; SUBJECT TO EASEMENTS AND RESTRICTIONS OF RECORD, IF ANY.

<u> P125644</u> --

THE NORTH HALF OF THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 27, TOWNSHIP 36 NORTH, RANGE 4 EAST, W.M., SKAGIT COUNTY, WASHINGTON:

SUBJECT TO AND TOGETHER WITH AND EASEMENT FOR INGRESS, EGRESS, AND UTILITIES OVER THE WEST 30 FEET OF THE WEST HALF OF THE SOUTHEAST QUARTER OF SECTION 27, TOWNSHIP 36 NORTH, RANGE 4 EAST, W.M., SKAGIT COUNTY, WASHINGTON;

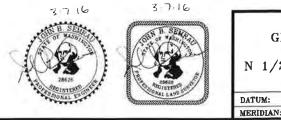
SUBJECT TO EASEMENTS AND RESTRICTIONS OF RECORD, IF ANY.

<u> P125645</u> -

THE SOUTH HALF OF THE NORTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 27, TOWNSHIP 36 NORTH, RANGE 4 EAST, W.M., SKAGIT COUNTY, WASHINGTON:

SUBJECT TO AND TOGETHER WITH AND EASEMENT FOR INGRESS, EGRESS, AND UTILITIES OVER THE WEST 30 FEET OF THE WEST HALF OF THE SOUTHEAST QUARTER OF SECTION 27, TOWNSHIP 36 NORTH, RANGE 4 EAST, W.M., SKAGIT COUNTY, WASHINGTON;

SUBJECT TO EASEMENTS AND RESTRICTIONS OF RECORD, IF ANY.



PRAISE RD

PU16-0092

APPLICANT/OWNER

CONCRETE NOR'WEST P.O. BOX 280 MOUNT VERNON, WA 98273 CONTACT: DAN COX TEL: (360) 757-3121

CIVIL ENGINEER/SURVEYOR SEMRAU ENGINEERING AND SURVEYING 2118 RIVERSIDE DRIVE SUITE 208 MOUNT VERNON, WA 98273 CONTACT: JOHN B. SEMRAU P.E. & P.L.S. TEL: (360) 424-9566

GEOTECHNICAL/GEOLOGICAL ENGINEER ASSOCIATED EARTH SCIENCES 2911 1/2 HEWITT AVENUE SUITE 2 EVERETT, WA 98201 CONTACT: CHUCK LINDSAY P.G., P.E.G. & P.Hg. TEL: (425) 259-0522

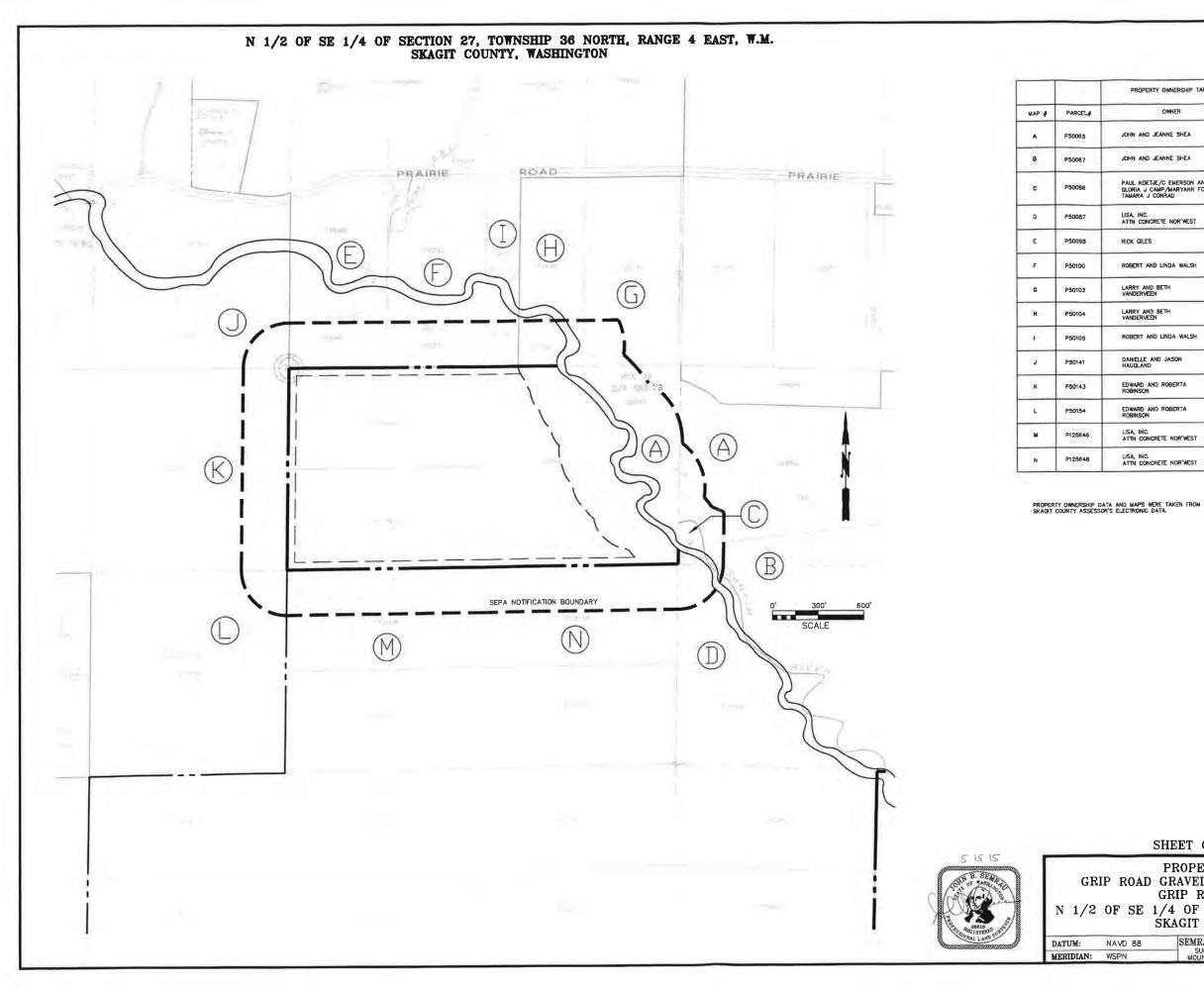
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EET NO.	DRAWING TITLE
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C1	CONTENTS AND VICINITY MAP
C2	PROPERTY OWNERSHIP MAP
C3	PRE-MINING TOPOGRAPHIC SURVEY MAP
C4	PROPOSED MINE CONTOURS
C5	RECLAMATION PLAN AND MINE SEQUENCE
C6	CROSS SECTIONS

JUN Z 4 2019 SKAGIT COUNTY PDS

SHEET C1 OF 7 PL16-0000

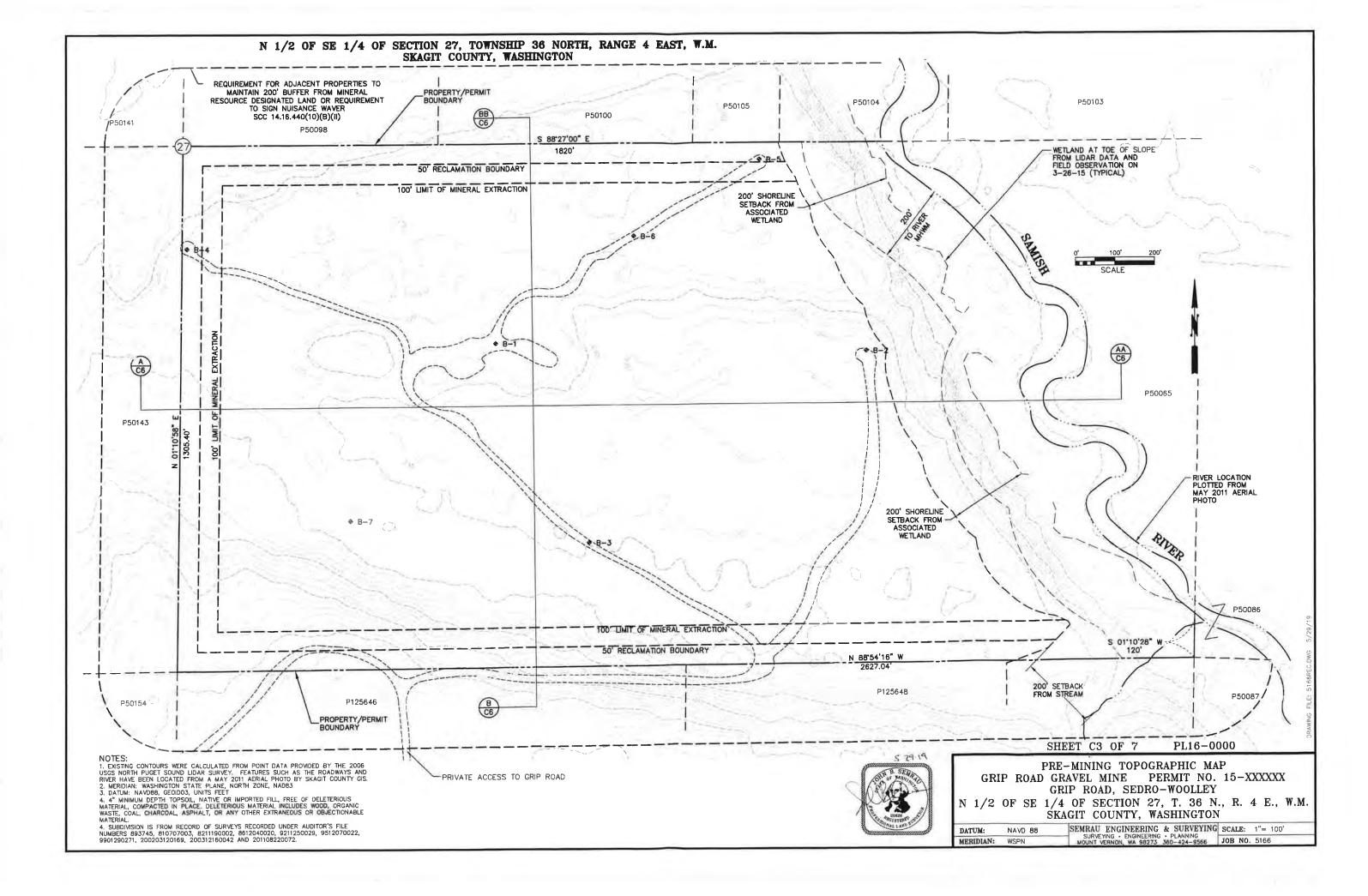
	P ROAD OF SE 1	CONTENTS AND VICINITY MAP GRAVEL MINE PERMIT NO. 16-XXXXXX GRIP ROAD, SEDRO-WOOLLEY /4 OF SECTION 27, T. 36 N., R. 4 E., W.M. SKAGIT COUNTY, WASHINGTON
	NAVD 88	SEMRAU ENGINEERING & SURVEYING SCALE: 1"= 600'
N:	WSPN	SURVEYING + ENGINEERING + PLANNING MOUNT VERNON, WA 98273 360-424-9566 JOB NO. 5166

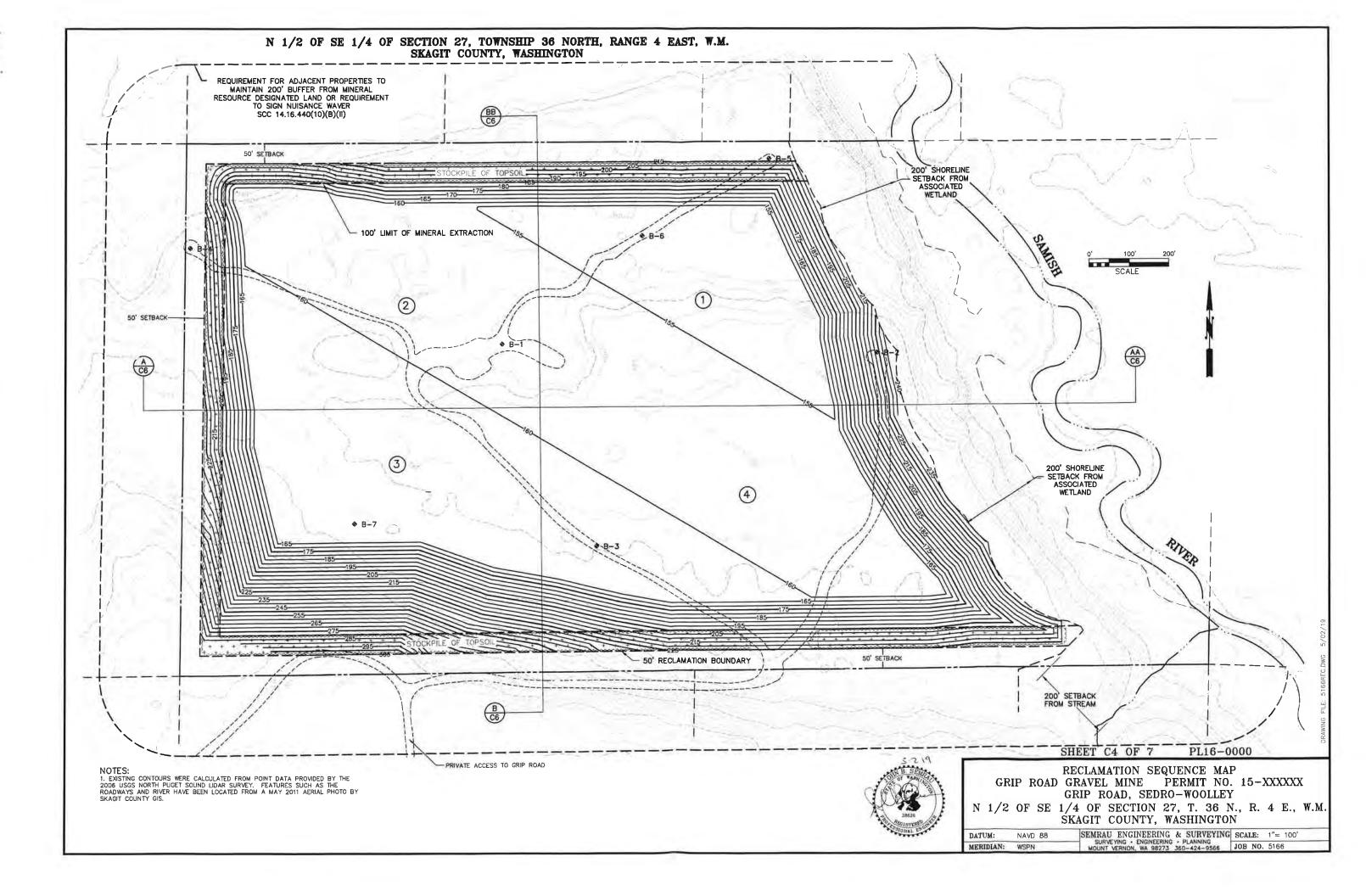


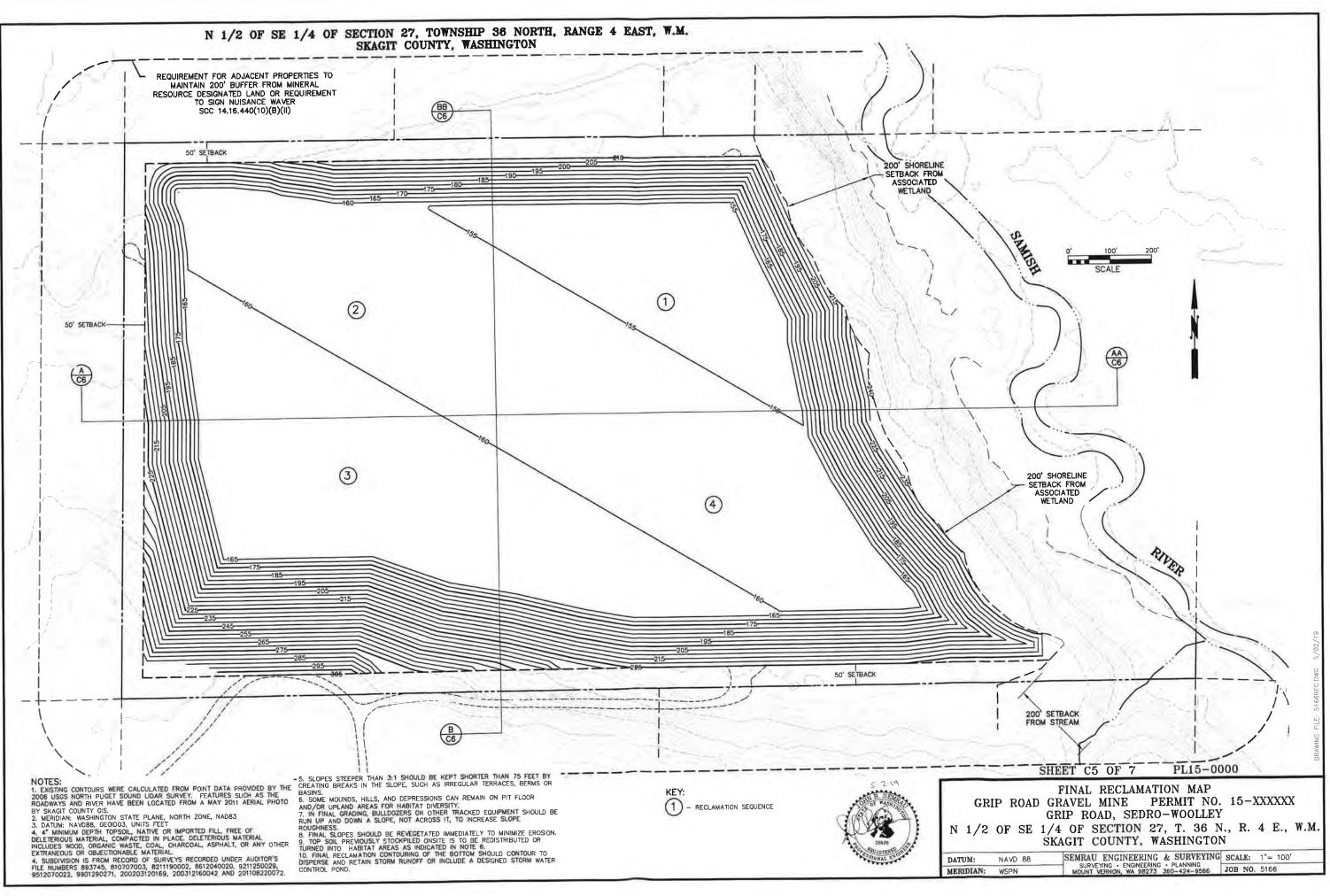
	PROPERTY OWNERSHIP TABLE	
	OWNER	OWNER ADDRESS
	JOHN AND JEANNE SHEA	P.O. BOX 311 BOW, WA. 88232
	JOHN AND JEANNE SHEA	P.D. BOX 311 BOW, WA. 98232
	PAUL KOETJE/C EMERSON AND GLORIA J CAMP/MARYANN FORD TAMARA J CONRAD	23318 127TH AVE NE ARLINGTON, WA 98223
T	LISA, INC. ATTN CONCRETE NOR'WEST	P.O. BOX 280 MOUNT VERNON, WA. 98273
	RICK GLES	21658 PRAIRIE RD. SEDRO-WOOLLEY, WA. 98284
	ROBERT AND LINDA WALSH	21710 PRAIRIE RD. SEDRO-WOOLLEY, WA. 98284
	LARRY AND BETH VANDERVEEN	21994 PRAIRIE RD. SEDRO-WOOLLEY, WA. 98284
	LARRY AND BETH VANDERVEEN	21994 PRAIRIE RD. SEDRO-WOOLLEY, WA. 98284
	ROBERT AND LINDA WALSH	21710 PRAIRIE RD. SEDRO-WOOLLEY, WA. 98284
	DANIELLE AND JASON HAUGLAND	21422 PRAIRIE RD: SEDRO-WOOLLEY, WA. 98284
	EDWARD AND ROBERTA ROBINSON	21000 PRAIRIE RD. SEDRO-WOOLLEY, WA. 98284
	EDWARD AND ROBERTA ROBINSON	21000 PRAIRIE RD. SEDRO-WOOLLEY, WA. 98284
	LISA, INC. ATTN CONCRETE NOR'WEST	F.O. BOX 280 MOUNT VERNON, WA. 98273
	USA, INC. ATTN CONCRETE NOR'WEST	P.O. BOX 280 MOUNT VERNON, WA. 98273

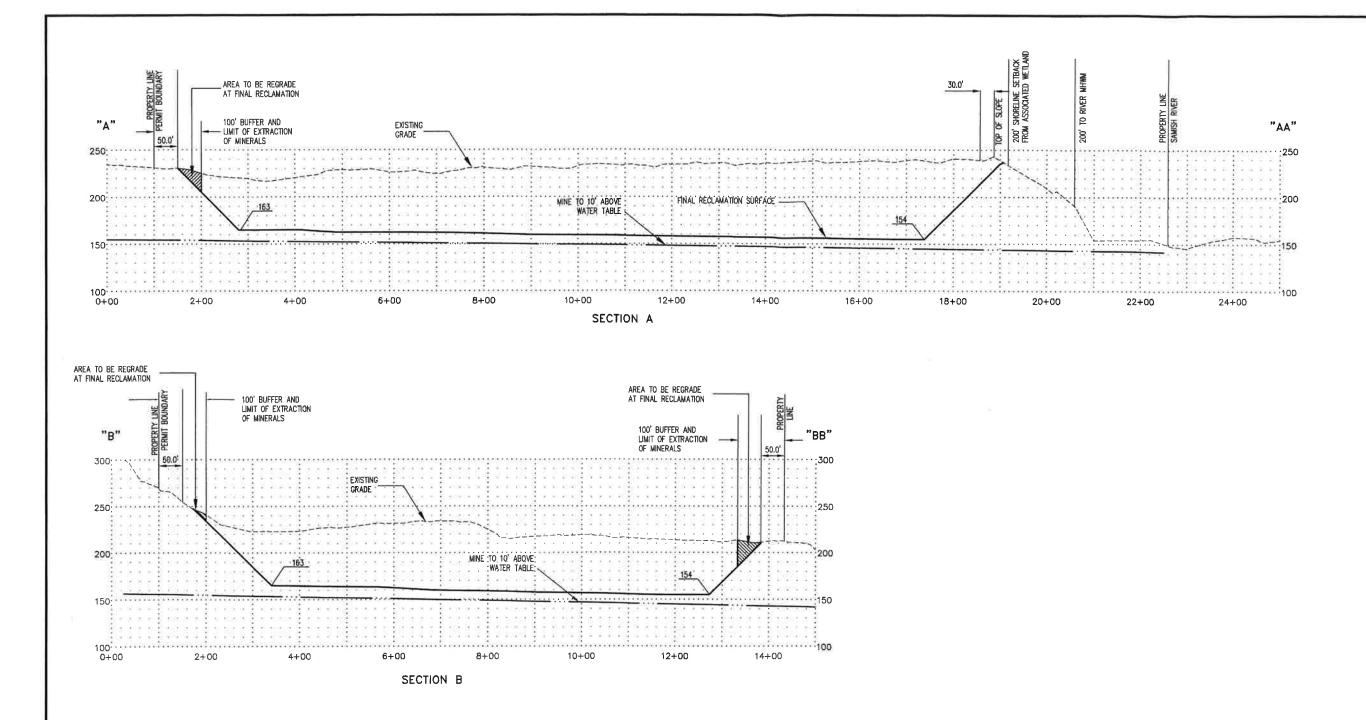
PL15-0000 SHEET C2 OF 7

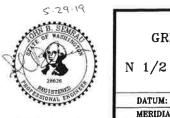
			GR GR 1/4	AVEI IP R OF	RTY MIN OAD, SECT COUL	IE SED TION	PE 0R0- 27,	RMI WO T.	F NO DLLE 36	O. EY N.				К W.M.
-	NAVD	88		SEMR	AU ENG	INEER	RING	& SU	RVEYI		SCALE:	: 1"=	= 300	o'
	WSPN	-		SURVEYING · ENGINEERIN				NG + PLANNING			JOB N	0, 516	66	





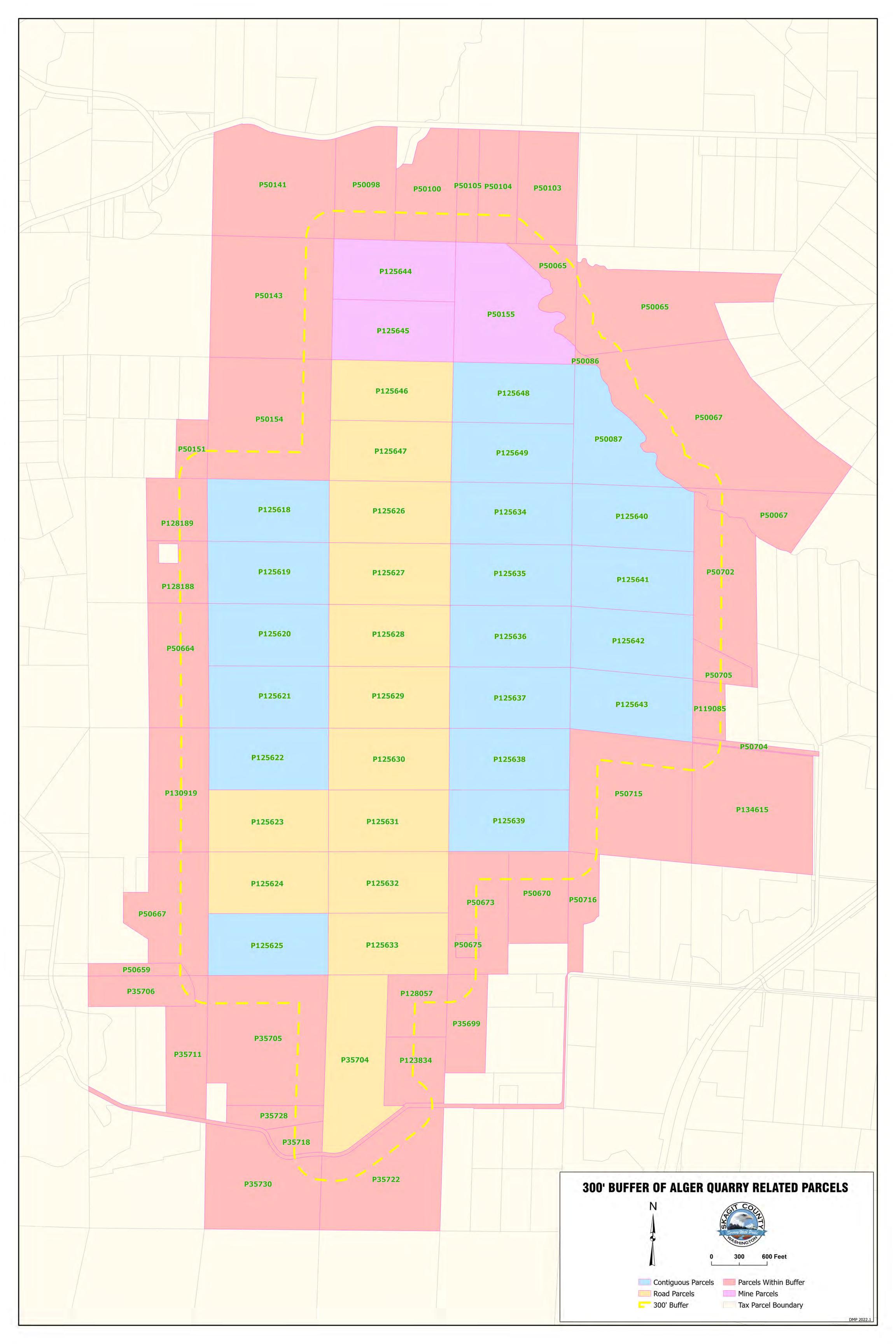






SHEET C6 OF 7 PL15-0000

			(CROSS	SECT	IONS				
RIP	ROA	DG	RAVEI	L MINE	P.	ERMIT	NO.	15-3	XXXX	X
			GRIP	ROAD,	BUR	LINGT	ON			
2 01	F SE	1/	4 OF	SECTION	ON 27	7, T.	36 N	I., R.	4 E.	, W.M.
				COUNT						
(:	NAVD	88	SEMR	U ENGIN	EERING	& SUR	VEYING	SCALE:	1"= 10	00'
IAN:	WSPN			SURVEYING . ENGINEE				JOB NO	516	6



PARCEL BREAKDOWN:

Mine:

- Parcel Owner
- P125644 Lisa INC.
- P125645
- P50155

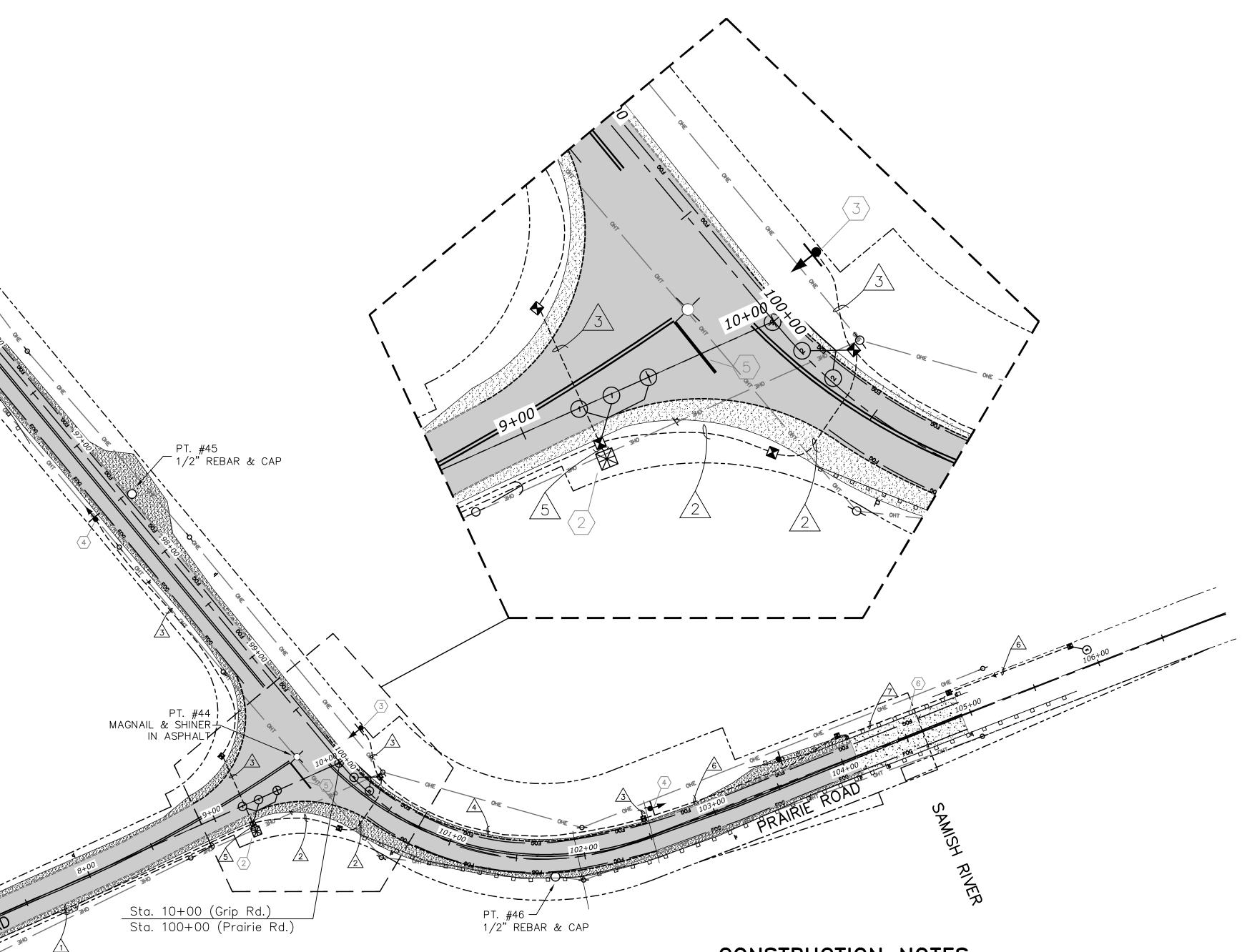
<u>Road:</u>

- P125646 Lisa INC.
- P125647
- P125626
- P125627
- P125628
- P125629
- P125630
- P125631
- P125623
- P125624
- P125632
- P125633
- P35704

Contiguous:

- P125648 Lisa INC.
- P125649
- P50087
- P125618
- P125634
- P125640
- P125619
- P125635
- P125641
- P125620
- P125636
- P125642
- P125621
- P125637
- P125643
- P125622
- P125638
- F125050
- P125639
- P125625

LEGEND Image: Stress of the stress of th	UNDATION		
 GENERAL NOTES 1. ALL WORK SHALL BE DONE IN ACCORSTANDARD SPECIFICATIONS, SKAGIT OF PLANS, AND THESE PLANS. 2. FLASHING BEACON SHALL HAVE 12 IN 3. CONDUIT SHALL BE SCH 40 UNLESS ALL CONDUIT SHALL HAVE 24 INCHESS 4. ALL VEHICLE DETECTION LOOPS SHALSTANDARD PLAN J-50.12-00. 5. ALL JUNCTION BOX LOCATIONS AND OF APPROVED BY ENGINEER BEFORE INS 6. STOP BAR MATERIAL SHALL BE THER 	COUNTY SPECIAL PROVISIONS, WSE NCH LENSE WITH LED LIGHT SOUR UNDER PAVEMENT WHERE IT SHAN S OF COVER. L BE WHERE SHOWN ON THE PLA CONDUIT RUNS ARE APPROXIMATE TALLATION.	OOT STANDARD CES. LL BE SCH 80. NS. SEE WSDOT	
<u>L</u> 1 <u>3S</u> 9+50.00, <u>0'</u> RT	CHEDULE Description Description	T RT	MA
DN TRAFFIC CONSULTANTS P.O. box 547 Preston, WA 98050 425.765.5721	PT. #47 MAGNAIL & SHINER IN ASPHALT	SAMISH RIVER	Image: Second
COPYRIGHT 2020 SEMRAU ENGINEERING & SURVEYING, PLLC CIVIL ENGINEERING • LAND SURVEYING • PLANNING 2118 RIVERSIDE DRIVE – SUITE 208 MOUNT VERNON, WA 98273 360-424-9566	VIAST DE LOS PORTOS	E REVISION	BY REV.



	WIRING SCHEDULE							
RUN NO.	CONDUIT RACEWAY	LOOP CABLE 2cs	FLASHER POWER #10	SERVICE #4	COMMENT			
1	1.5"			3	120/240 VAC BY PSE			
2	2"	2	5		SCH 80			
3	1"		3		SCH 80			
4	1"	1	3					
5	2"	3	7					
6	1"	1						
7	1.5"	1			GALVANIZED			

CONS	TRU	

CONNECT SERVICE POWER CONNECTION CABINET AT STA.
 CONSTRUCT FOUN CABINET AT STA.
 CONSTRUCT FOUN TYPE 1" AT STA. AND J-21.10-04.
 CONSTRUCT FOUN TYPE 1" AT STA. DETAIL C AND E.
 REMOVE EXISTING
 ALL CONDUIT OVE

ALL CONDUIT OVER BRIDGE BETWEEN JUNCTION BOXES SHALL BE 1.5" GALVANIZED, INCLUDING ALL 90 DEGREE BENDS (R=6" MIN). CONDUIT SHALL BE ATTACHED TO BACKSIDE OF GUARDRAIL POSTS USING GALVANIZED 2-HOLE CLAMPS AND SHALL BE ATTACHED TO EACH POST, AND TWO CLAMPS AT EACH RISER 16" OC. SEE SHEET 2 DETAIL D.

PRAIRIE RD & GRIP RD INTERSECTION TRAFFIC IMPROVEMENTS SCALES: HORIZONTAL : 1" = 50' VERTICAL : N/A NOTE: IF THIS SHEET IS LESS THAN 22"x 34" THEN SHEET HAS BEEN REDUCED

G

ION NOTES

CONNECT SERVICE TO TRANSFORMER AT STA. 5+25.97, 19.28' RT. COORDINATE POWER CONNECTION WITH PSE.

CONSTRUCT FOUNDATION, FURNISH AND INSTALL TYPE PS POLE AND FLASHER CABINET AT STA. 9+24.00, 21' RT. SEE SHEET 2 DETAIL A.

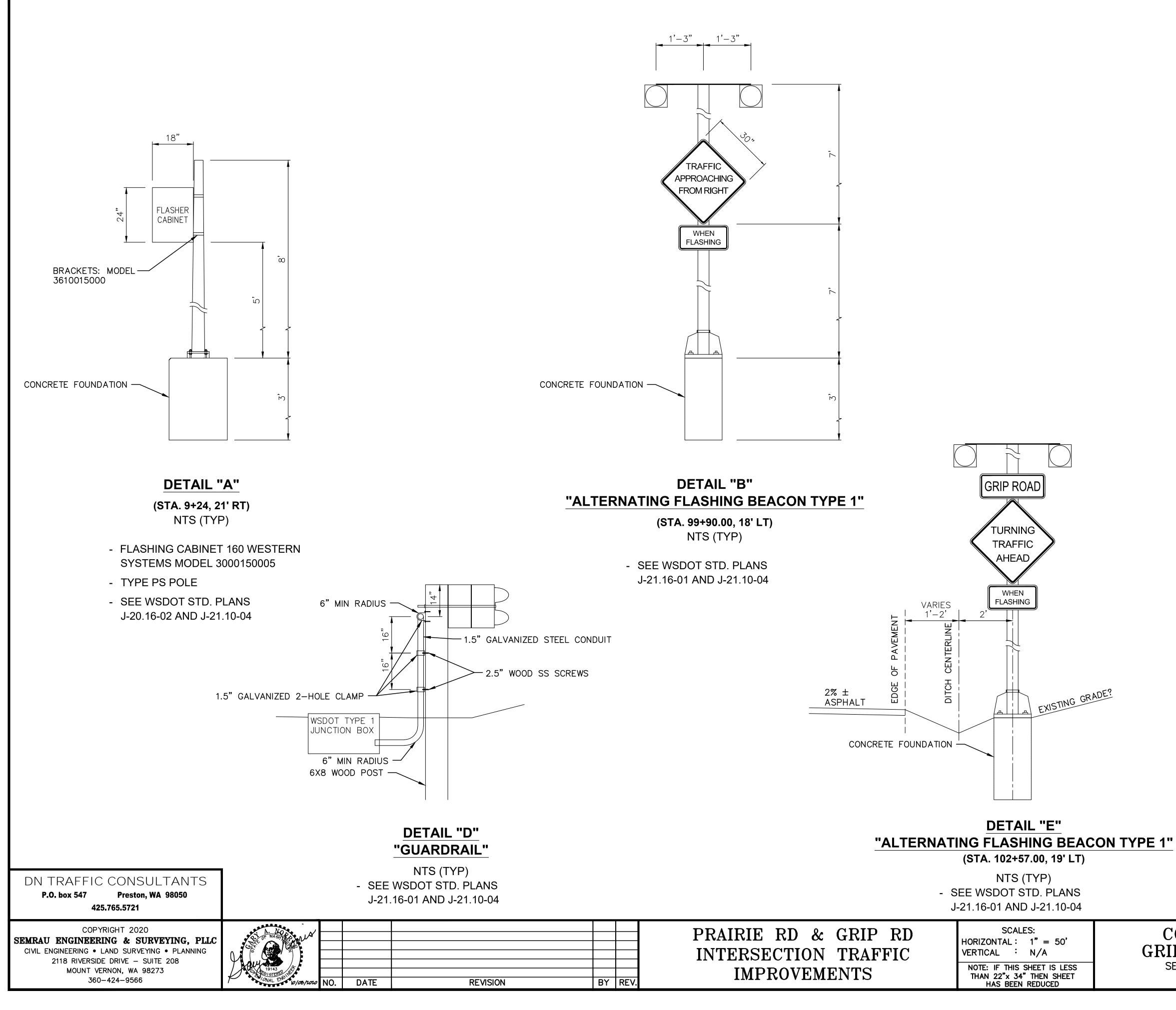
CONSTRUCT FOUNDATION, FURNISH AND INSTALL "ALTERNATING FLASHING BEACON TYPE 1" AT STA. 99+90.00, 18' LT. SEE WSDOT STANDARD PLAN J-21.16-01 AND J-21.10-04. SEE SHEET 2 DETAIL B.

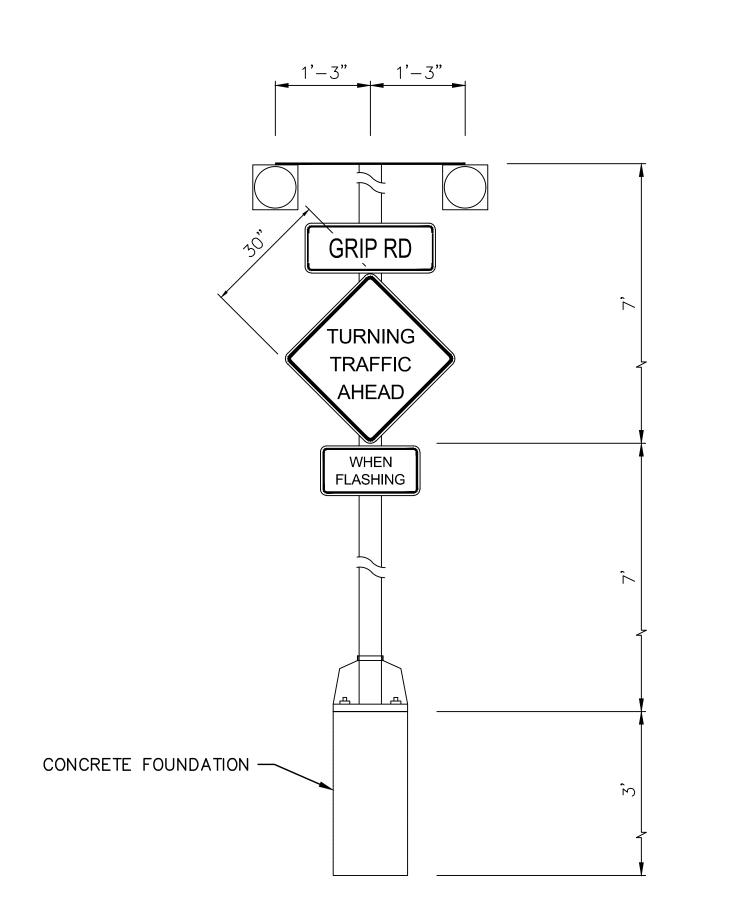
CONSTRUCT FOUNDATION, FURNISH AND INSTALL "ALTERNATING FLASHING BEACON TYPE 1" AT STA. 102+57.00, 15'LT AND STA 97+50, 20' RT. SEE SHEET 2 DETAIL C AND E.

REMOVE EXISTING STOP BAR AND INSTALL NEW 12" STOP BAR AT STA. 9+66.74.

	FIELD BK
	DATE:
GRIP ROAD GRAVEL MINE	DRAWING:
SECTION 4, T. 35 N., R. 4 E., W.M.	JOB NO .:
	SHEET:

FIELD BK/PG:					
DATE:	8/27/20				
DRAWING:	5166FBD.DWG				
JOB NO .:	5166				
SHEET:	1				

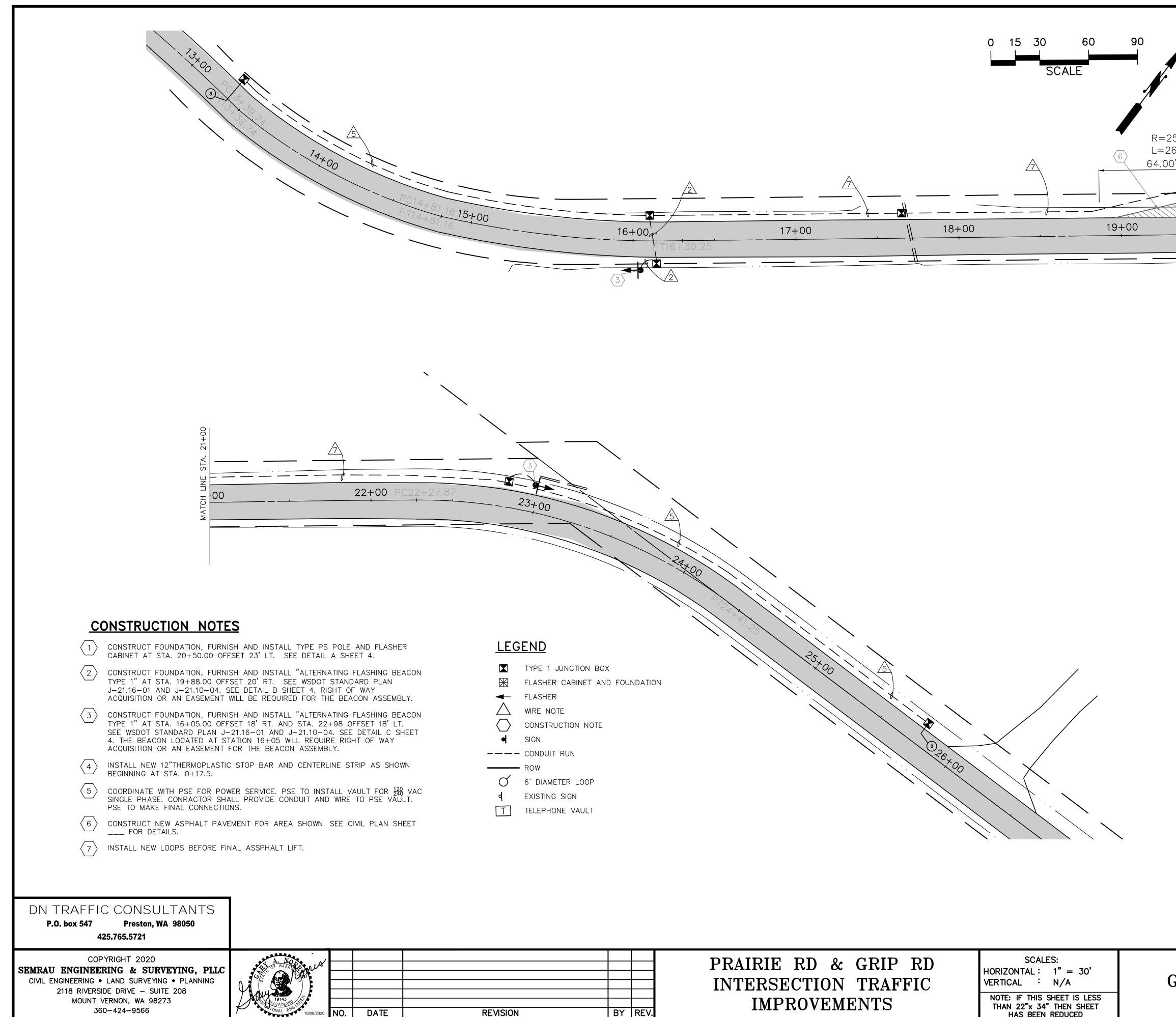




DETAIL "C" "ALTERNATING FLASHING BEACON TYPE 1" (STA. 97+50, 20' RT) NTS (TYP)

- SEE WSDOT STD. PLANS J-21.16-01 AND J-21.10-04

CONCRETE NOR'WEST	FIELD BK/PG	
	DATE:	8/27/2020
GRIP ROAD GRAVEL MINE	DRAWING: 516	66FBD.DWG
SECTION 4, T. 35 N., R. 4 E., W.M.	JOB NO.: 516	66
	SHEET:	2



BY	REV.

PRAIRIE	RD	&	GRIP	RD
INTERSE	CTI	ON	TRAF	FIC
IMP	ROV	EMI	ENTS	

HAS BEEN REDUCED

25 26.05' 0' 22.00' 0' STOP BAR 8 20 00 21+ 0' 21+ 0 21+ 0' 21+ 0 21+ 0' 21+ 0' 21+ 0 2 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 2 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21+ 0 21 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	
BAR 8 20 00 21+ 2 2 2 2 2 2 2 2 2 2 2 2 2	

	LOOP SCHEDULE						
RD	LOOP#	TYPE	STATION OFFSET	SS	LOOP#	TYPE	STATION OFFSET
GRIP	2	ЗA	25+85.00, 5'LT	ACCE	1	3S	0+75.00, 5'LT
1	3	3A	13+20.00 5'RT	MINE			

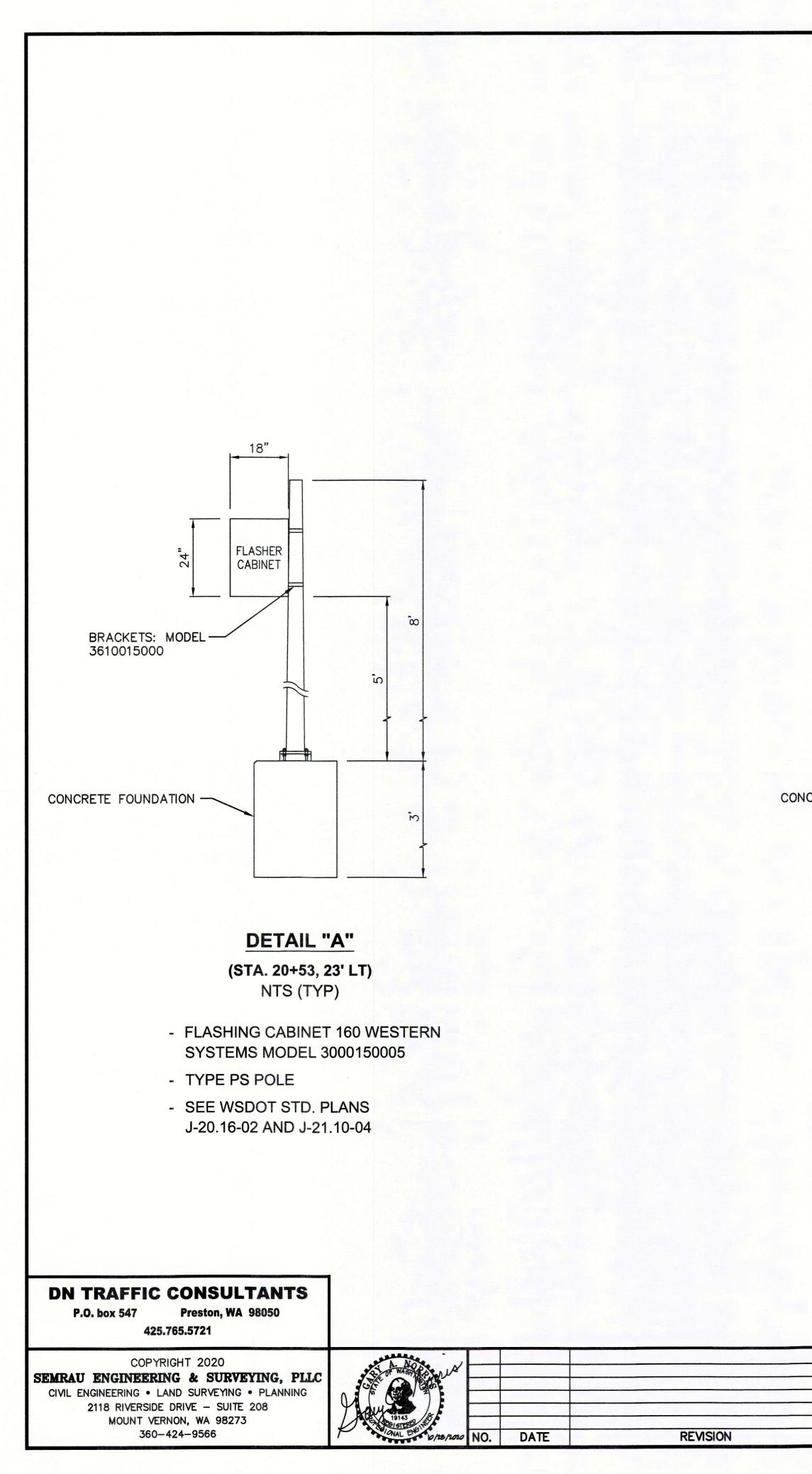
NOTE: STATION AND OFFSET REFERENCED TO THE LEADING LOOP CENTER POINT FOR ALL TYPE 3S LOOPS.

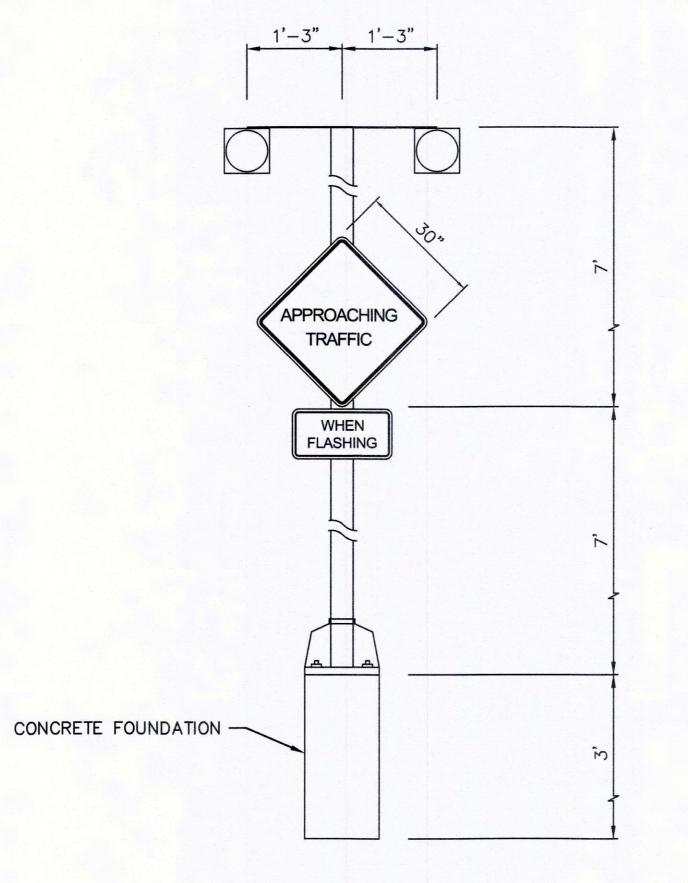
		WIR	NG SC	HEDU	LE
RUN NO.	CONDUIT RACEWAY	LOOP CABLE 2cs	FLASHER POWER #10	CABINET SERVICE #4	COMMENT
1	1.5"			3	120/240 VAC BY PSE
2	1.5"		3		SCH 80
3	1.5"		5		SCH 80
4	1.5"	2	5		
5	1.5"	1			
6	1.5"	2	5		
7	1.5"	1	3		
8	2"	3	5	3	

GENERAL NOTES

- 1. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE CURRENT WSDOT/APWA STANDARD SPECIFICATIONS, SKAGIT COUNTY SPECIAL PROVISIONS, WSDOT STANDARD PLANS, AND THESE PLANS.
- 2. FLASHING BEACON SHALL HAVE 12 INCH LENSE WITH LED LIGHT SOURCES.
- 3. CONDUIT SHALL BE SCH 40 UNLESS UNDER PAVEMENT WHERE IT SHALL BE SCH 80. ALL CONDUIT SHALL HAVE 24 INCHES OF COVER.
- 4. ALL VEHICLE DETECTION LOOPS SHALL BE WHERE SHOWN ON THE PLANS. SEE WSDOT STANDARD PLAN J-50.12-00.
- 5. ALL JUNCTION BOX LOCATIONS AND CONDUIT RUNS ARE APPROXIMATE AND SHALL BE APPROVED BY ENGINEER BEFORE INSTALLATION.
- 6. STOP BAR MATERIAL SHALL BE THERMOPLASTIC.
- 7. FLASHER CABINET SHALL BE WESTERN SYSTEMS FC 160; MODEL #3000150005. BRACKETS ARE MODEL 3610015000.

CONCRETE NOR WEST	FIELD BK/PG:	
	DATE:	
GRIP ROAD GRAVEL MINE	DRAWING:	5166FBD-3.DWG
SECTION 4, T. 35 N., R. 4 E., W.M.	JOB NO .:	5166
	SHEET:	3





DETAIL "B" "ALTERNATING FLASHING BEACON TYPE 1"

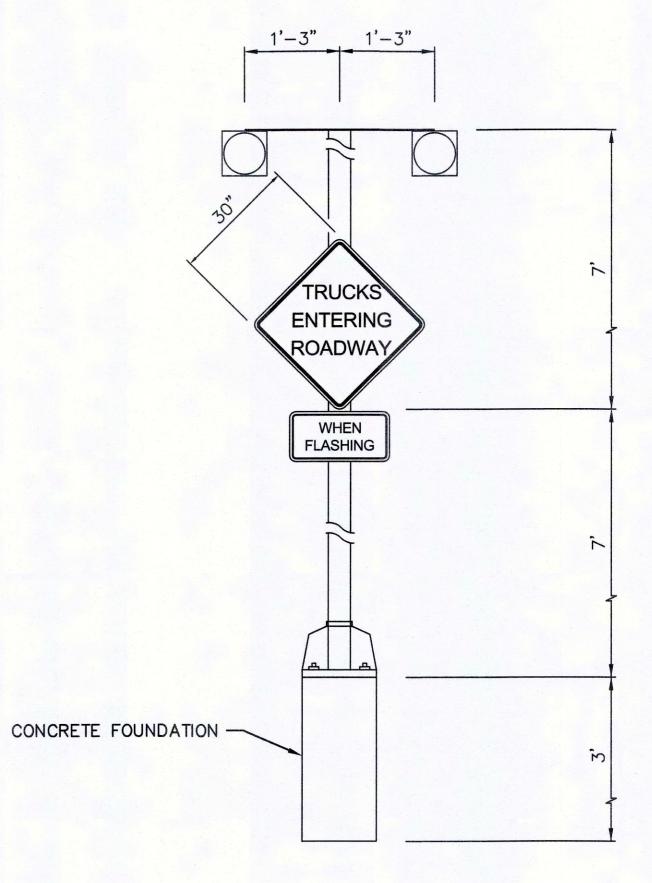
(STA. 19+88.00, 20' RT) NTS (TYP)

- SEE WSDOT STD. PLANS J-21.16-01 AND J-21.10-04

A Market	BY	REV.
		1.00

PRAIRIE	RD	&	GRIP	RD
INTERSE	CTI	ON	TRAF	FIC
IMPROVEMENTS				

SC	AL	ES:
ORIZONTAL	:	1" = 50'
ERTICAL	:	N/A
		SHEET IS LESS
		THEN SHEET



DETAIL "C" "ALTERNATING FLASHING BEACON TYPE 1" (STA. 16+05, 18' RT) (STA. 22+98, 18' LT) NTS (TYP)

> - SEE WSDOT STD. PLANS J-21.16-01 AND J-21.10-04

CONCRETE NOR'WEST	FIELD BK/PG:	
	DATE: 10/28/2020	
	DRAWING: 5166FBD.DWG	
	JOB NO.: 5166	
SKAGIT COUNTY, WASHINGTON	SHEET: 4	

SITE VISIT (4/15/2022) SITE PHOTOS:



Entrance to "Haul Road" (behind gate) located off of Gripp Road (a County roadway). Haul Road is a private roadway that is used to access the subject properties and proposed mine.





Looking toward entrance /exit with Gripp Road in distance.



Gripp Road near entrance /exit to Haul Road



Gripp Road near entrance /exit to Haul Road



Private internal Haul Road. Around corner in distance is private bridge.



Internal bridge



Haul Road. Very remote, wooded, and secluded.











